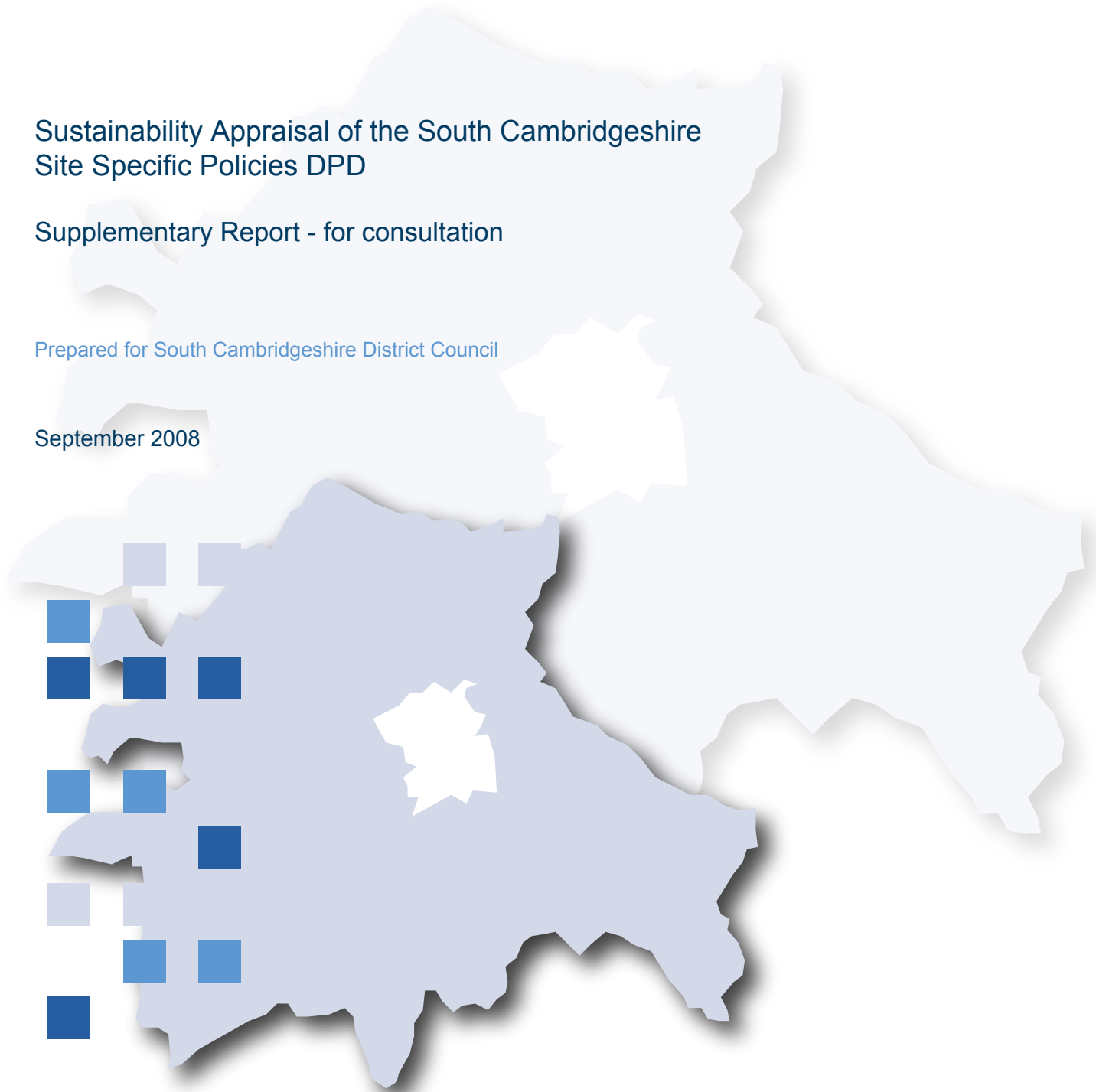


# Sustainability Appraisal of the South Cambridgeshire Site Specific Policies DPD

Supplementary Report - for consultation

Prepared for South Cambridgeshire District Council

September 2008



## Revision Schedule

### Sustainability Appraisal of the South Cambridgeshire Site Specific Policies DPD – Supplementary Report September 2008

Rev	Date	Details	Prepared by	Reviewed by	Approved by
01	September 2008	Draft report for consultation	<b>Mark Fessey</b> Assistant Environmental Consultant	<b>Alex White</b> Senior Consultant	<b>Steve Smith</b> Associate

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**Scott Wilson**  
Scott Wilson Ltd.  
Third Floor  
6 – 8 Greencoat Place  
London  
SW1P 1PL  
T. +44 (0) 2077985000  
W. [www.scottwilson.com](http://www.scottwilson.com)

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# Non-Technical Summary

## Introduction

This section provides a non-technical summary of the findings set out in this Supplementary Sustainability Appraisal (SA) Report. The non-technical summary for the SA Report should, according to the SEA Directive and transposing Regulations provide:

**a non-technical summary of the information provided under the above headings.**

- (a) an outline of the contents, main objectives of the plan or programme and relationship with other relevant plans and programmes;
- (b) the relevant aspects of the current state of the environment and the likely evolution thereof without implementation of the plan or programme;
- (c) the environmental characteristics of areas likely to be significantly affected;
- (d) any existing environmental problems which are relevant to the plan or programme including, in particular, those relating to any areas of a particular environmental importance, such as areas designated pursuant to Directives 79/409/EEC and 92/43/EEC;
- (e) the environmental protection objectives, established at international, Community or Member State level, which are relevant to the plan or programme and the way those objectives and any environmental considerations have been taken into account during its preparation;
- (f) the likely significant effects (1) on the environment, including on issues such as biodiversity, population, human health, fauna, flora, soil, water, air, climatic factors, material assets, cultural heritage including architectural and archaeological heritage, landscape and the interrelationship between the above factors;
- (g) the measures envisaged to prevent, reduce and as fully as possible offset any significant adverse effects on the environment of implementing the plan or programme;
- (h) an outline of the reasons for selecting the alternatives dealt with, and a description of how the assessment was undertaken including any difficulties (such as technical deficiencies or lack of know-how) encountered in compiling the required information; and
- (i) a description of the measures envisaged concerning monitoring in accordance with Article 10.

SEA Directive Annex I

## Sustainability Appraisal

### What is Sustainability Appraisal?

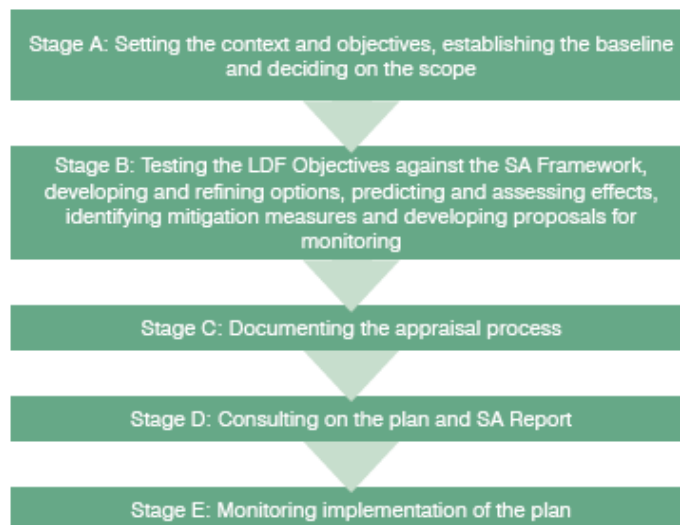
SA is the process of assessing the likely impacts of a plan on the environment, the community and the economy. By law, an SA must be carried out of plans that set out the spatial planning requirements of a local authority. At the moment, local authorities are in the process of preparing new plans which will guide spatial planning in their district for the next few years - the Local Development Framework (LDF). The LDF consists of a number of Local Development Documents (LDDs). There are two types of LDDs – Development Plan Documents (DPDs) and Supplementary Planning Documents (SPDs). DPDs and SPDs set out local authority priorities for the area as a whole (e.g. the Core Strategy DPD), as well as area specific policies (e.g. Development Brief SPDs).

European Directive 2001/42/EC also requires certain plans to undergo a Strategic Environmental Assessment (SEA) to ascertain their effects on the environment. This Directive was incorporated into UK law in 2004. The Government recommends that the two processes of SA and SEA are integrated and simply referred to as SA. The Government has produced guidance for undertaking both processes which is available here:

<http://www.communities.gov.uk/publications/planningandbuilding/sustainabilityappraisal>

## How was it done?

Government Guidance separates the SA process into five stages. These five stages run in parallel to the development of the DPD. The figure below shows the different stages of the SA process.



Stages A and B are key stages of the SA process. The outcomes of Stage A – the scoping stage - were set out in a Scoping Report, published in 2006 and available on the council's website<sup>1</sup>. The evidence gathered and the scope decided upon at Stage A is utilised to guide and focus the assessment stage (Stage B).

Stage B - the assessment stage - has been an iterative process. The draft DPD was first assessed in 2005, with the findings set out in the Final SA Report submitted alongside the Site Specific Policies DPD in January 2006. There is now a need to amend the draft DPD, and therefore a need to undertake further SA. The findings of this second round of SA are set out in this Supplementary SA Report.

<sup>1</sup> Available at: [http://www.scams.gov.uk/documents/retrieve.htm?pk\\_document=3616](http://www.scams.gov.uk/documents/retrieve.htm?pk_document=3616) (Accessed 09/08)

## The South Cambridgeshire Site Specific Policies DPD

The Site Specific Policies DPD was submitted to the Secretary of State in January 2006 as one of a set of Local Development Documents prepared by South Cambridgeshire District Council (SCDC) as part of the development of their Local Development Framework. This plan was subject to Sustainability Appraisal (SA) and accompanied by a Final SA Report<sup>2</sup>.

At the Examination in Public of the DPD (held between November 2007 and January 2008) the Inspector concluded that there was a housing shortfall of 1,600 dwellings against the Core Strategy target of 20,000 new dwellings between 1999 and 2016. In addition, there has been a change in circumstances with regard to the Cambridge Northern Fringe East Site, where a recent report confirms that the relocation of the Cambridge Sewage Treatment Works and redevelopment of the area for housing-led development is not financially viable. As such, the proposed housing allocation for 600 dwellings at Chesterton Sidings (the part of the area in South Cambs) can no longer be relied upon. This therefore increased the housing shortfall from 1,600 to 2,200 dwellings.

The Inspector subsequently asked the Council to undertake a comparative assessment of Objection Sites (sites put forward by objectors to the plan) and to consult on assessment findings, before identifying how the shortfall can best be met. The Inspector also recognised that further SA would be required to support the comparative assessment and consultation.

In order to identify how best to meet the housing shortfall in the district, the Council tested to ensure that reasonable site options had been identified, and undertook a first stage assessment of the 35 sites identified to see if they passed fundamental planning considerations. Those that failed this assessment were rejected at this stage. The 16 sites that passed that first stage were then subject to full comparative assessment, and have been tested in this SA. Furthermore, the appraisal has also considered the relative merits of the North West Cambridge Area Action Plan site. The complete list of plan options is set out below:

- Parcel L2, Arbury Park
- Parcel Com 4, Arbury Park
- Parcel Q and H.R.C.C, Arbury Park
- North West Cambridge AAP
- Land between Huntingdon Road, Histon Road and the A14, North West Cambridge (Objection Site)
- Land between Huntingdon Road, Histon Road and the A14, North West Cambridge (South Cambs Revision)
- Land North of Barton Road
- Land north of Fen Road, Milton (Greengates Piggeries)

<sup>2</sup> Available at: [http://www.scambs.gov.uk/documents/retrieve.htm?pk\\_document=3611](http://www.scambs.gov.uk/documents/retrieve.htm?pk_document=3611) (Accessed 09/08)

- Land south of Manor Park / Somerset Road
- Powell's garage, Woollards Lane
- The Ida Darwin Hospital
- Land between Teversham Road and Cow Lane
- Land West of Cambourne
- Land North of A428
- Bourn Airfield (1)
- Bourn Airfield (2)

## The SA Objectives

A list of sustainability objectives were developed following the review of the locally specific sustainability baseline and context (the evidence) at the scoping stage. These objectives are collectively known as the SA framework. The framework sets the boundaries and scope of the assessment and the objectives provide the yardstick against which the likely significant effects of the plan have been predicted and evaluated. The full list of sustainability objectives are set out below.

Sustainability appraisal objectives	
1.1	Minimise the irreversible loss of undeveloped land and productive agricultural holdings
1.2	Reduce the use of non-renewable resources, including energy sources
1.3	Limit water consumption to levels supportable by natural processes and storage systems
2.1	Avoid damage to designated sites and protected species
2.2	Maintain and enhance the range and viability of characteristic habitats and species
2.3	Improve opportunities for people to access and appreciate wildlife and wild places
3.1	Avoid damage to areas and sites designated for their historic interest, and protect their settings.
3.2	Maintain and enhance the diversity and distinctiveness of landscape and townscape character
3.3	Create places, spaces and buildings that work well, wear well and look good
4.1	Reduce emissions of greenhouse gasses and other pollutants (including air, water, soil, noise, vibration and light)
4.2	Minimise waste production and support the recycling of waste products
4.3	Limit or reduce vulnerability to the effects of climate change (including flooding)
5.1	Maintain and enhance human health
5.2	Reduce and prevent crime, and reduce the fear of crime
5.3	Improve the quantity and quality of publicly accessible open space
6.1	Improve the quality, range and accessibility of services and facilities (e.g. health, transport, education, training, leisure opportunities)



6.2	Redress inequalities related to age, gender, disability, race, faith, location and income
6.3	Ensure all groups have access to decent, appropriate and affordable housing
6.4	Encourage and enable the active involvement of local people in community activities
7.1	Help people gain access to satisfying work appropriate to their skills, potential and place of residence
7.2	Support appropriate investment in people, places, communications and other infrastructure
7.3	Improve the efficiency, competitiveness, vitality and adaptability of the local economy

## The findings of the Sustainability Appraisal

Each of the 16 sites was assessed against the SA framework, with the aim of highlighting the relative sustainability strengths and weaknesses of each location, and therefore aiding the process of deciding upon which should be selected as a 'preferred option'. In order to aid this comparative assessment a summary table of the findings of the stand-alone appraisals has been prepared.

The options appraisal was a qualitative exercise based on professional judgement on the part of Scott Wilson taking into account the information gathered in the Scoping Report. For each option, the significance of effects in terms of each SA objective was evaluated by means of a commentary and also given a broad categorisation using the scoring system as shown below.

### *Appraisal scoring definitions<sup>3</sup>*

<b>++</b>	Option will result in a positive impact on the SA Objective
<b>+</b>	The impact on the SA Objective is dependant on implementation, but if there were to be an impact it would most likely be positive.
<b>0</b>	Neutral or negligible effect
<b>-</b>	The impact on the SA Objective is dependant on implementation, but if there were to be an impact it would most likely be negative.
<b>--</b>	Option will result in a negative impact on the SA Objective

Set out below is the options appraisal summary table.

<sup>3</sup> N.B. In some cases, differing symbols have been used to score the appraisal. Where this occurs the **first** symbol takes priority. For example an -? score will technically be a negative score, with some uncertainty, a ?- score will technically be a uncertain score but with the potential to be negative.

*Options appraisal summary table*

Objective	Sub-objective	Parcel L2, Arbury Park	Parcel Com 4, Arbury Park	Parcel Q and H.R.C, Arbury Park	North West Cambridge AAP	Huntingdon /, Histon Road / A14, North West Cambridge	Huntingdon /, Histon Road / A14, North West Cambridge (South Cambs Revision)	land north of Barton Rd.	Land north of Fen Road, Milton (Greengates Piggeries)	Powell's garage	Ida Darwin Hospital	Teversham Road / Cow Lane	Land south of Manor Park / Somerset Road	Land West of Cambourne	Land North of A428	Bourne Airfield (1)	Bourne Airfield (2)
Land and water resources	Undeveloped land	+?	+?	+?	-	-	-	-	?	+	+	-	-	-	-	?	?
	Non-renewable resources	0	0	0	+?	0	0	0	0	0	0	0	0	0	0	0	0
	Water	0	0	0	0	0	0	0	0?	0	-?	-?	0	0?	0?	0?	0?
Biodiversity	Designated sites and species	+	+	+	?	?	?	-?	+	+	+	+	+	0?	-?	-?	-?
	Characteristic habitats and species	+	+	+	?	?	?	?-	?	+?	+	?-	?	0-	?	-?	-?
	Access to wildlife and wild places	?	?	?	+	+?	+?	+	++	0	+	+	0	+?	+	+?	+?

Landscape, townscape and archaeology	Historic interest	+	+	?	?-	?	?	+	?	?	+	?-	?	-?	-?	-?	-?
	Landscape and townscape	+	-?	+	--	--	-	-	-?	?	?	-?	-?	-	-	-	-
	Places, spaces and buildings	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Climate change and pollution	Greenhouse gasses and other pollutants	++	++	++	+	+	+	-?	?	+	+	?	?+	-	-	-	-
	Waste	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
	Vulnerability to climate change	+	+	+	?	?-	?+	--	-	+	?	?	+	?	?	?	?
Healthy communities	Health	-	-	-	?	-	?	-	-	0	0+	-	-?	0	0	?-	?-
	Crime, and fear of crime	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
	Publicly accessible open space	+	+	+	+	?+	?	?	?	0	+	0+	-	+	+	+	+
Inclusive communities	Services and facilities	+	+	+	++	+	+	?	-	+	?	+	+	-	-	?-	-?
	Inequalities	0	0	0	0	0	0	0	?-	0	0	0	0	0	0	0	0
	Housing	+	+	+	++	+	+	+	+	+	+	+	+	+	+	+	+
	Community	+	+	+	+	0	0	0	0?	0	0	0	0	0	?-	?-	?
Economic activity	Access to satisfying work	+	+	+	+	+	+	0?	+	?	+	+	+	?	-	?	?



	Investment in people, places and infrastructure	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
	Local economy	0	0	0	0	0	0	0	0	0	0+	0	0	+	0	0	0

SCDC have taken account of this process of sustainability appraisal as they have chosen their preferred options – those sites that they believe are best placed to make up the housing shortfall in the district. For each preferred option SCDC have drafted a policy that aims to guide future development at that location, recognising the local constraints and opportunities. The policies prepared for each of the preferred options have also undergone a process of SA. Rather than a comparative appraisal, the aim was to appraise each policy against the SA framework so as to determine the actual effects of implementing that Policy. As with all SA, recommendations have been made as to how the policies might be improved to build upon any weaknesses and build upon strengths.

Appraisal summaries have been prepared that identify where the policies perform well (i.e. where they take account of locational constraints / opportunities to effectively mitigate negative effects of development / make the most of sustainability opportunities resulting from development). There is also a focus on identifying the significant residual sustainability effects that will remain, assuming that the Policy is fully implemented. Where possible recommendations have been made as to how residual effects might be mitigated / enhanced. However, it is recognised that many negative residual effects are currently very uncertain, and so as well as making further recommendations for Policy changes, detailed ideas have been set out with regard to what effects the Council may wish to monitor.

Set out below are the appraisal summaries for each of the Council's preferred options:

*Land between Huntingdon Road, Histon Road and the A14, North West Cambridge (South Cambs revision)*

**Summary:**

This is a detailed policy which focuses on ensuring that a number of the key sustainability issues presented major development at this location are addressed. In particular, the Policy focuses on mitigating landscape impacts associated with the loss of an important area of Green Belt and ensuring water efficiency and minimising flood-risk. Traffic congestion and air / noise pollution associated with the A14 is also a constraint that is given careful consideration by the Policy. The Policy also seeks to most the most of locational opportunities by promoting an integrated approach to delivering biodiversity and high quality landscapes alongside the provision of open space and countryside access. This is a location that has the potential to allow for sustainable transport patterns amongst residents. The Policy seeks to build on this potential by stating what will be delivered on the ground in terms of public transport and high quality cycling and walking routes.

Nonetheless, there will be some residual negative sustainability impacts that cannot be wholly mitigated by the Policy. Foremost is the issue regarding the loss of productive agricultural land in the Green Belt and the impacts of this on the landscape setting of Cambridge. Effects may be particularly severe during the construction phase, reducing over time as a result of landscaping. Similarly, development at this location will put strain on water resources and potentially lead to negative effects in terms of air quality and noise objectives, but effects will be mitigated to some degree by the requirements of the Policy.

**Recommendations:**

1. Clarify why a 'balance may need to be struck' in terms of the provision of affordable housing.
2. Demonstrate that services and facilities will be adequately accessible for all members of society, including the elderly and disabled, from the day that people move into the new

development.

3. Add clarity to this requirement for exemplar developments

### *Powell's Garage, Great Shelford*

**Summary:**

This is a small scale redevelopment within the village of Great Shelford. The Policy aims to ensure that the two major locational constraints will be adequately addressed as part of future development, namely the integrity of the conservation area and the potential issue of land contamination. It is possible that there may be some negative residual effects on the integrity of the conservation area, although good design will go a long way to mitigating effects and could even result in positive effects in terms of townscape. A high density of development is proposed to make the most of the fact that this is a brownfield location with good access to local services and facilities as well as public transport. Because of the relatively modest size of any potential development, the Policy is silent on all other issues, instead relying on Development Control Policies DPD policies to ensure that potential sustainability issues are addressed.

**Recommendations:**

None

### *Ida Darwin Hospital, Fulbourn*

**Summary:**

This Policy has a focus on addressing the key locational constraint, which is the presence of the site within the Cambridge Green Belt. Development at this site, as guided by this Policy can be considered as a re-configuration of the Major Developed Site in the Green Belt, as a green wedge will be created where there is currently built development, providing compensatory open space with a countryside character. The Policy promotes the opportunity for the green wedge to lead to biodiversity gain and improved access to green space for the residents of Fulbourn village. The Policy refers to the requirement for a biodiversity strategy, and the supporting text highlights that the reversion of land to open countryside presents opportunities for biodiversity enhancement. The Policy and/or supporting text also refer to other key constraints to development at this location, namely land contamination, flood risk, the designated conservation area and noise and vibration from the nearby railway. There area unlikely to be any significant residual negative sustainability effects associated with these constraints, assuming that the Policy and other Development Control Policies DPD policies are fully implemented. Even though this is not a small development, there are a number of other issues that are not elaborated on in the Policy, but rather Development Control Policies DPD policies will be utilised to ensure that there are no negative sustainability impacts and that sustainability benefits are maximised.

**Recommendations:**

None

### *Arbury Park, Cambridge*

**Summary:**

This is a Policy to guide a large mixed-use development. The Policy promotes making the most of opportunities for sustainable transport and ecological enhancement. It takes a range of important constraints into detailed account, particularly the mitigation of noise and emissions from the A14, the need to protect the landscape setting Cambridge, the need to minimise traffic congestion and the need to protect the Arbury Camp site of archaeological interest. There are significant local constraints in terms of traffic congestion and associated noise and air quality impacts. Implementation of the Policy will go some distance to mitigating effects, and it is likely that the A14 improvements will improve the situation, but it will be important to monitor to ensure no residual effects, particularly in terms of health the health of residents. Even though this is a large development, there are a number of other issues that are not elaborated on in the Policy, but rather Development Control Policies DPD policy only will be utilised to ensure that there are no negative sustainability impacts and that sustainability benefits can be maximised.

**Recommendations:**

None

## Summary of the effects of the plan

The following section discusses the likely sustainability effects of the implementation of the DPD. It considers the effects of bringing forward the preferred options in combination with the sites identified in the previous submission Site Specific Policies DPD. The discussion in this section is not exhaustive, and should not be read in isolation, but rather is intended to summarise some of the key sustainability benefits, dis-benefits and trade-offs identified through the SA.

There are clear socio-economic benefits at the Cambridge scale of further development at the preferred Objection Sites. Further development will go some way to meeting the demand for housing and affordable housing, and new housing for employees is important if the Cambridge economy is to continue to prosper. This SA Report has highlighted that delivering further growth at the preferred sites will result in only limited significant trade-offs in terms of other socio-economic SA objectives. It does not appear that existing communities will be significantly impacted by development at any of the sites (although there may be some potential for impacts during the construction phase of development).

However, the choice of several of the sites has raised questions as to whether there may be the potential for the health of new residents to suffer as a result of air and noise pollution. Several of the sites also have the potential to impact on already significant levels of traffic congestion, particularly on the A14. However, the plan does seek to mitigate these potential effects, and so significant residual effects cannot be predicted with any certainty at this stage, but must instead be monitored closely where possible.

Development on this scale at the preferred Objection Sites will inevitably result in a range of trade-offs needing to be made in terms of important environmental SA objectives. Significant development will come forward within the Cambridge Green Belt and therefore there is the potential for local biodiversity baseline to be affected and for the landscape setting of Cambridge to be eroded. Plan policies do seek to mitigate these effects, and in the case of biodiversity in

particular, if the issue remains an important consideration throughout implementation of the plan, then it is likely that significant residual effects can be in many ways avoided.

Another key impact is related to the fact that, almost no-matter how well designed houses are to ensure water efficiency, further development in and around Cambridge will put significant additional strain on the already stretched water resources of the East of England. The previously submitted Final SA Report also highlighted this effect, and so it is clear that bringing forward the preferred Objection Sites in addition to wider development will exacerbate pressure on the region's water resources.

Another residual environmental effect, the significance of which is somewhat uncertain, is the potential to increase flood-risk (primarily off-site by contributing to increased run-off, as none of the proposed sites are themselves at immediate risk of fluvial flooding). With all environmental effects (flood risk is only one example) it will be important to consider the potential for negative sustainability effects to act cumulatively.

Development at the proposed sites may be considered to have some key sustainability benefits. In particular, the preferred sites all perform well in terms of increasing the potential for residents to adopt more sustainable transport patterns. In this sense, and given that the business as usual scenario would be likely to involve people commuting longer distances into Cambridge, development at these locations is likely to allow for a decrease in the per capita carbon footprint of new residents. However, it will be important that this opportunity for significant sustainability benefits is capitalised upon through appropriate investment and other incentives to change unsustainable behaviour.

A final key sustainability consideration of the revised DPD relates to the proposal to delete policy SP/2 of the original plan. The effect of this change will be to lose an allocation of 600 dwellings in the North East of the City, but instead to safeguard the land for the development of a railway station and interchange facility. Transport interchanges of this type (this interchange will link with the Cambridge Guided Busway) will be important in order to achieve a City-wide renaissance in the way that people travel.

## Contact Information

For further information please contact:

Planning Policy Team  
South Cambridgeshire District Council  
South Cambridgeshire Hall  
Cambourne Business Park  
Cambourne  
Cambridge  
CB23 6EA

tel: 08450 450 500  
fax: 01954 713152  
website: [www.scambs.gov.uk](http://www.scambs.gov.uk)



# 1 Introduction

## 1.1 The South Cambridgeshire Site Specific Policies DPD

- 1.1.1 The Site Specific Policies DPD was submitted to the Secretary of State in January 2006 as one of a set of Local Development Documents prepared by South Cambridgeshire District Council (SCDC) as part of the development of their Local Development Framework (LDF). This plan was subject to Sustainability Appraisal (SA) and accompanied by a Final SA Report<sup>4</sup>.
- 1.1.2 At the Examination in Public of the DPD (held between November 2007 and January 2008) the Inspector concluded that there was a housing shortfall of 1,600 dwellings against the Core Strategy target of 20,000 new dwellings between 1999 and 2016. In addition, there has been a change in circumstances with regard to the Cambridge Northern Fringe East Site, where a recent report confirms that the relocation of the Cambridge Sewage Treatment Works and redevelopment of the area for housing-led development is not financially viable. As such, the proposed housing allocation for 600 dwellings at Chesterton Sidings (the part of the area in South Cambs) can no longer be relied upon. This therefore increased the housing shortfall from 1,600 to 2,200 dwellings.
- 1.1.3 The Inspector subsequently asked the Council to undertake a comparative assessment of Objection Sites (sites put forward by objectors to the plan) and to consult on assessment findings, before identifying how the shortfall can best be met. The Inspector also recognised that further SA would be required to support the comparative assessment and consultation.
- 1.1.4 Scott Wilson have been commissioned by SCDC to undertake the SA of this stage of the plan-making process.

## 1.2 This SA Report

- 1.2.1 This Report is a supplement to the SA Report that was consulted on and submitted alongside the submitted Site Specific Policies DPD in 2005/06. As such, this Supplementary SA Report should be read alongside the Final SA Report, and in a number of places, rather than repeating background information, the reader is signposted to the Final SA Report.

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<sup>4</sup> Available at: [http://www.scams.gov.uk/documents/retrieve.htm?pk\\_document=3611](http://www.scams.gov.uk/documents/retrieve.htm?pk_document=3611) (Accessed 09/08)

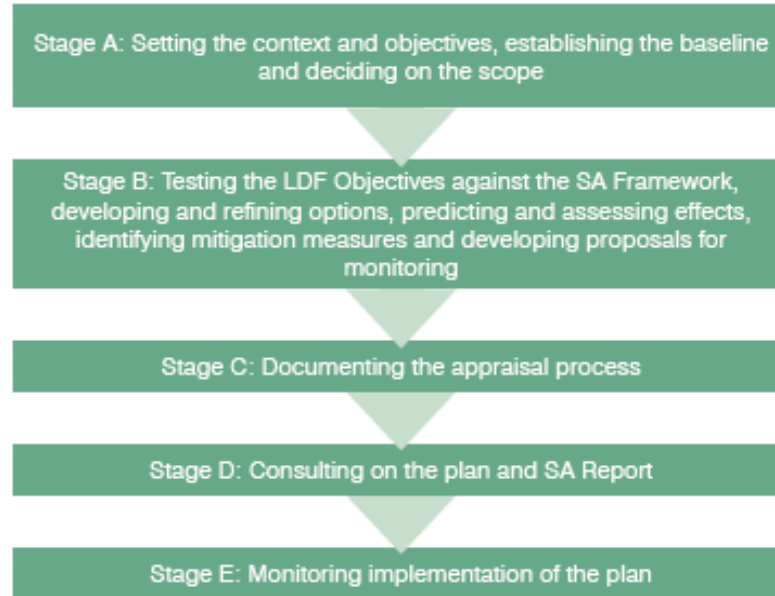
## 1.3 Sustainability Appraisal and Strategic Environmental Assessment

- 1.3.1 Under the Planning and Compulsory Purchase Act (PCPA) (2004), each Local Development Document (LDD) – the components of a Local Development Framework (LDF) - must undergo a Sustainability Appraisal (SA). SA involves the identification and evaluation of the LDD's impacts on economic, social and environmental objectives – i.e. its compatibility with the three dimensions of sustainable development. The SA process incorporates the requirements of a European law requiring certain plans and programmes to undergo a formal Strategic Environmental Assessment (SEA).
- 1.3.2 SEA involves the systematic identification and evaluation of the environmental impacts of a strategic action (e.g. a plan or programme). In 2001, the EU legislated for SEA with the adoption of Directive 2001/42/EC on the assessment of the effects of certain plans and programmes on the environment (the 'SEA Directive'). The Directive entered into force in the UK on 21 July 2004 and applies to a range of English plans and programmes including many LDDs.
- 1.3.3 SA and SEA are therefore both statutory requirements. The Government's approach to this dual requirement is to incorporate the requirements of the SEA Directive into a the SA process. The SA, which considers all three pillars of sustainability, therefore incorporates the requirements of the SEA Directive, but does not have any bias towards giving consideration to environmental issues. To this end, the Government published guidance<sup>5</sup> on undertaking SA of LDDs ('the Guidance') in November 2005. The combined SA / SEA process is referred to in the Guidance and in this document as 'Sustainability Appraisal (SA)'.
- 1.3.4 The Guidance advocates a five-stage approach to undertaking SA (see **Figure 1.1**).

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<sup>5</sup> Available from: <http://www.communities.gov.uk/publications/planningandbuilding/sustainabilityappraisal>

*Figure 1.1: Five stage SA process.*



- 1.3.5 Stages A and B are key stages of the SA process. The outcomes of Stage A – the scoping stage - were set out in a Scoping Report, published in 2006 and available on the council’s website<sup>6</sup>. The evidence gathered and the scope decided upon at Stage A is utilised to guide and focus the assessment stage (Stage B).
- 1.3.6 Stage B - the assessment stage - has been an iterative process. The draft plan was first assessed in 2005, with the findings set out in the Final SA Report submitted alongside the Site Specific Policies DPD in January 2006. Following the findings of the Planning Inspectors and other changes in local circumstances, that DPD now requires amendments to meet a housing shortfall, and so there is a need to undertake further assessment. The findings of this second round of assessment are set out in this Supplementary SA Report.
- 1.3.7 The production of the Final SA Report and this Supplementary SA Report together meet the requirements of Stage C of the SA process.
- 1.3.8 This Supplementary SA Report will be subject to consultation alongside the revised Site Specific Policies DPD, therefore meeting the requirements of Stage D of the SA process. A key aim of the SA Report is to support the consultation on the DPD by allowing stakeholders to make more informed consultation responses. The final stage – Stage E – involves meeting the requirement to monitor the implementation of the plan. This SA Report sets out proposals for key plan effects that should be monitored.

<sup>6</sup> Available at: [http://www.scams.gov.uk/documents/retrieve.htm?pk\\_document=3616](http://www.scams.gov.uk/documents/retrieve.htm?pk_document=3616) (Accessed 09/08)

## 1.4 Report structure

1.4.1 This report is structured as follows:

- **Section 1** – Sets the context of the DPD and the Sustainability Appraisal
- **Section 2** – Summarises the findings of Stage A of the SA process (the ‘scoping’ stage’)
- **Section 3** – Sets out the methodology, appraisal findings monitoring proposals and difficulties encountered
- **Section 4** – Describes what will happen next in the plan-making process

## 1.5 Meeting the requirements of the SEA Directive

1.5.1 The SEA Directive sets out a legal assessment process that must be followed. In light of this, this report clearly sets out the relevant requirements of the SEA Directive and explains how these have been satisfied (or will be satisfied).

### Signposting

For ease of access, where the SEA Directive or Regulations require a specific task to be completed, or information to be present, there will be signposted boxes, similar to this, that highlight which aspects of the Directive and Regulations have been met by that section

1.5.2 **Table 1.1** below indicates where specific requirements of the SEA Directive can be found within this Supplementary SA Report and the Final SA Report.

*Table 1.1: SEA Directive requirements checklist*

Environmental Report requirements <sup>7</sup>	Section of this report
an outline of the contents, main objectives of the plan or programme and relationship with other relevant plans and programmes;	<ul style="list-style-type: none"> <li>• Supplementary SA Report Section 1.1</li> </ul>
the relevant aspects of the current state of the environment and the likely evolution thereof without implementation of the plan or programme;	<ul style="list-style-type: none"> <li>• SCDC LDF Sustainability Appraisal Scoping Report (2006)</li> <li>• Final SA Report</li> <li>• Supplementary SA Report Section 2</li> </ul>

<sup>7</sup> As listed in Annex I of the SEA Directive (Directive 2001/42/EC on the assessment of the effects of certain plans and programmes on the environment).

the environmental characteristics of areas likely to be significantly affected;	<ul style="list-style-type: none"> <li>• SCDC LDF Sustainability Appraisal Scoping Report (2006)</li> <li>• Final SA Report</li> </ul>
any existing environmental problems which are relevant to the plan or programme including, in particular, those relating to any areas of a particular environmental importance, such as areas designated pursuant to Directives 79/409/EEC and 92/43/EEC;	<ul style="list-style-type: none"> <li>• SCDC LDF Sustainability Appraisal Scoping Report (2006)</li> <li>• Final SA Report</li> </ul>
the environmental protection objectives, established at international, Community or Member State level, which are relevant to the plan or programme and the way those objectives and any environmental considerations have been taken into account during its preparation;	<ul style="list-style-type: none"> <li>• SCDC LDF Sustainability Appraisal Scoping Report (2006)</li> <li>• Final SA Report</li> </ul>
the likely significant effects <sup>8</sup> on the environment, including on issues such as biodiversity, population, human health, fauna, flora, soil, water, air, climatic factors, material assets, cultural heritage including architectural and archaeological heritage, landscape and the interrelationship between the above factors;	<ul style="list-style-type: none"> <li>• Final SA Report</li> <li>• Supplementary SA Report Section 3</li> </ul>
the measures envisaged to prevent, reduce and as fully as possible offset any significant adverse effects on the environment of implementing the plan or programme;	<ul style="list-style-type: none"> <li>• Final SA Report</li> <li>• Supplementary SA Report Section 3</li> </ul>
an outline of the reasons for selecting the alternatives dealt with, and a description of how the assessment was undertaken including any difficulties (such as technical deficiencies or lack of know-how) encountered in compiling the required information;	<ul style="list-style-type: none"> <li>• Supplementary SA Report Section 1 and 3</li> </ul>
a description of the measures envisaged concerning monitoring in accordance with Article 10;	<ul style="list-style-type: none"> <li>• Final SA Report</li> <li>• Supplementary SA Report Section 3</li> </ul>
a non-technical summary of the information provided under the above headings.	<ul style="list-style-type: none"> <li>• Supplementary SA Report</li> </ul>

<sup>8</sup> These effects should include secondary, cumulative, synergistic, short, medium and long-term permanent and temporary, positive and negative effects.

## 2 Stage A

### 2.1 Introduction

2.1.1 Stage A in the SA process involves five key tasks:

- **A1** – Identify other relevant plans, programmes and sustainability objectives that should influence the LDF and be considered as part of the SA.
- **A2** – Collect relevant social, environmental and economic baseline information
- **A3** – Identify key sustainability issues specific to the LDF to be considered as part of the SA
- **A4** – Develop the SA framework, consisting of the SA objectives, indicators and targets
- **A5** – Produce a Scoping Report and consult relevant authorities, the public and other key stakeholders on the scope of the appraisal and evidence base collected.

2.1.2 Stage A involves collecting evidence that can inform the development a set of locally specific sustainability objectives and other decision-making criteria (the ‘SA framework’). Furthermore, the evidence collected at Stage A is then available to be made use of more generally to inform the appraisal of the draft plan against the SA framework. The SA framework and evidence base were documented in a Scoping Report, which was subject to consultation in late 2005 and is now available on the council’s website.<sup>9</sup>

2.1.3 As agreed with SCDC, the Scoping Report (2006) has been the key source of information used to guide and inform this SA. In addition to this, further evidence used to inform the plan-making process has been made available by SCDC, and has been drawn upon in order to inform the SA.

### 2.2 Context Review (A1)

2.2.1 The first task in Stage A of the SA process involved reviewing the policy and sustainability context in which the South Cambridgeshire District Council LDF is being prepared. This entailed reviewing a large number of policies, plans, programmes, strategies and initiatives prepared at international, national, regional and local level considered relevant to the LDF. This review identified a number of pre-requisites (including targets) that LDF policies must reflect in the light of local circumstances. The context review is documented in the LDF Scoping Report and the documents reviewed are also listed in the Final SA Report.

<sup>9</sup> Available at: [http://www.scams.gov.uk/documents/retrieve.htm?pk\\_document=3616](http://www.scams.gov.uk/documents/retrieve.htm?pk_document=3616) (Accessed 09/08)

## 2.3 Baseline information (A2)

- 2.3.1 Research was undertaken to assemble a baseline dataset which quantifies local conditions on 40 parameters, including river water quality, air quality, loss of high quality agricultural land, the area and condition of important wildlife habitats, housing completion rate and the achievement of energy efficiency ratings in new dwellings, levels and patterns of commuting and travel to school, availability of shops and other amenities in the District's villages, unemployment levels, educational achievement rates, etc. Data on conditions in adjacent local authority areas, in the East of England, or nationally, was used to determine whether environmental, economic and social conditions in the District were favourable, average or typical of the surrounding region, or unsatisfactory and in need of specific corrective policy.
- 2.3.2 The findings of the baseline review are documented in the Scoping Report. Updated baseline information can be found in the South Cambridgeshire District Council Annual Monitoring Report.<sup>10</sup>

## 2.4 Sustainability Issues (A3)

- 2.4.1 Stage A3 involves taking account of the evidence gathered at stages A1 and A2 to identify the main sustainability issues for South Cambridgeshire. These sustainability issues are listed in the Scoping Report and repeated in the Final SA Report, under the headings of:
- Land and water resources
  - Biodiversity
  - Landscape, townscape & archaeology
  - Climate change and pollution
  - Healthy communities
  - Inclusive communities
  - Economic activity

## 2.5 SA objectives (A4)

- 2.5.1 SA is fundamentally based on an objectives-led approach whereby the potential impacts of a plan are gauged in relation to a series of objectives for sustainable development. In other words, the objectives provide a methodological yardstick against which to assess the effects of the plan.

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<sup>10</sup> Available at:  
[http://www.scambs.gov.uk/Environment/Planning/DistrictPlanning/LocalDevelopmentFramework/Annual\\_Monitoring\\_Report.htm](http://www.scambs.gov.uk/Environment/Planning/DistrictPlanning/LocalDevelopmentFramework/Annual_Monitoring_Report.htm)  
(Accessed 09/08)

2.5.2 As part of Stage A of the SA process, a series of 22 sustainable development objectives and other decision-making criteria (the SA framework) were established. The SA framework is detailed in the LDF Scoping Report, as well as also being set out in the Final SA Report and **Table 2.1** below.

*Table 2.1: The SA framework*

Sustainability topic	Sustainability appraisal objectives	Decision-making criteria
Land and water resources	1.1 Minimise the irreversible loss of undeveloped land and productive agricultural holdings	Will it use land that has been previously developed?
		Will it use land efficiently?
		Will it protect and enhance the best and most versatile agricultural land?
	1.2 Reduce the use of non-renewable resources, including energy sources	Will it reduce emissions of greenhouse gases by reducing energy consumption?
		Will it lead to an increased proportion of energy and other resources being met from renewable sources?
	1.3 Limit water consumption to levels supportable by natural processes and storage systems	Will it reduce water consumption?
Will it conserve ground water resources?		
Biodiversity	2.1 Avoid damage to designated sites and protected species	Will it protect sites designated for nature conservation interest?
	2.2 Maintain and enhance the range and viability of characteristic habitats and species	Will it conserve species, reversing declines, and help to enhance diversity?
		Will it reduce habitat fragmentation?
		Will it help achieve Biodiversity Action Plan targets?
	2.3 Improve opportunities for people to access and appreciate wildlife and wild places	Will it improve access to wildlife, and wild places?
		Will it maintain and, where possible, increase the area of high-quality green space in the District?
		Will it promote understanding and appreciation of wildlife?
Will it improve access to the wider countryside through the network of public rights of way?		



Sustainability topic	Sustainability appraisal objectives	Decision-making criteria
Landscape, townscape and archaeology	3.1 Avoid damage to areas and sites designated for their historic interest, and protect their settings.	Will it protect or enhance sites, features of areas of historical, archaeological, or cultural interest (including conservation areas, listed buildings, registered parks and gardens and scheduled monuments)?
	3.2 Maintain and enhance the diversity and distinctiveness of landscape and townscape character	Will it maintain and enhance the diversity and distinctiveness of landscape and townscape character?
		Will it protect and enhance open spaces of amenity and recreational value?
		Will it maintain and enhance the character of settlements?
	3.3 Create places, spaces and buildings that work well, wear well and look good	Will it improve the satisfaction of people with their neighbourhoods as places to live?
		Will it lead to developments built to a high standard of design, and good place making?
Climate change and pollution	4.1 Reduce emissions of greenhouse gasses and other pollutants (including air, water, soil, noise, vibration and light)	Will it reduce emissions of greenhouse gases?
		Will it improve air quality?
		Will it reduce traffic volumes?
		Will it support travel by means other than the car?
		Will it reduce levels of noise or noise concerns?
		Will it reduce or minimise light pollution?
	4.2 Minimise waste production and support the recycling of waste products	Will it reduce household waste?
		Will it increase waste recovery and recycling?
	4.3 Limit or reduce vulnerability to the effects of climate change (including flooding)	Will it minimise risk to people and property from flooding, storm events or subsidence?
Healthy communities	5.1 Maintain and enhance human health	Will it substantially reduce mortality rates?
		Will it encourage healthy lifestyles, including travel choices?
5.2 Reduce and	Will it reduce actual levels of crime?	

Sustainability topic	Sustainability appraisal objectives	Decision-making criteria
	prevent crime, and reduce the fear of crime	Will it reduce fear of crime?
	5.3 Improve the quantity and quality of publicly accessible open space	Will it increase the quantity and quality of publicly accessible open space?
Inclusive communities	6.1 Improve the quality, range and accessibility of services and facilities (e.g. health, transport, education, training, leisure opportunities)	Will it improve the quality and range of services and facilities, including health, education, shopping, sport, leisure, arts and cultural activities?
		Will it improve accessibility to key local services and facilities, including health, education and leisure (shops, post offices, pubs etc)?
		Will it improve accessibility by means other than the car and improve the attractiveness of environmentally better modes including public transport, cycling and walking?
		Will it support and improve community and public transport?
	6.2 Redress inequalities related to age, gender, disability, race, faith, location and income	Will it improve relations between people from different backgrounds or social groups?
		Will it reduce poverty and social exclusion in those areas most affected?
		Will it promote accessibility for all members of society, including the elderly and disabled?
	6.3 Ensure all groups have access to decent, appropriate and affordable housing	Will it support the provision of a range of housing types and sizes, including affordable and key worker housing, to meet the identified needs of all sectors of the community?
		Will it reduce the number of unfit homes?
		Will it meet the needs of the travelling community?
6.4 Encourage and enable the active involvement of local people in community activities	Will it increase the ability of people to influence decisions?	
	Will it encourage engagement with community activities?	
Economic activity	7.1 Help people gain access to satisfying work appropriate to their skills, potential and place of residence	Will it encourage businesses development?
		Will it improve the range of employment opportunities to provide a satisfying job or occupation for everyone who wants one?
		Will it improve accessibility to local employment by means other than the car?

Sustainability topic	Sustainability appraisal objectives	Decision-making criteria
		Will it encourage the rural economy and diversification?
	7.2 Support appropriate investment in people, places, communications and other infrastructure	Will it improve the level of investment in key community services and infrastructure?
		Will it support provision of key communications infrastructure, including broadband?
		Will it improve access to education and training, and support provision of skilled employees to the economy?
	7.3 Improve the efficiency, competitiveness, vitality and adaptability of the local economy	Will it improve business development and enhance competitiveness?
		Will it support the Cambridge area's position as a world leader in research and technology based industries, higher education and research, particularly through the development and expansion of clusters?
		Will it support sustainable tourism?
		Will it protect the shopping hierarchy, supporting the vitality and viability of Cambridge City Centre, town, district, and local centres?

## 3 Stage B

### 3.1 Introduction

The SEA Directive requires the environmental assessment to identify, describe and evaluate'...'*the likely significant effects on the environment of a plan or programme*' Annex II of the SEA Directive provides criteria which help determine the likely significance of an effect.

**SEA Directive, Article 5 & Annex II**

The Guidance states that the 'purpose of this task is to predict the social, environmental and economic effects of the options being considered in the DPD process'. Also, the Guidance states that 'having identified and described the likely effects of the DPD, an evaluation of their significance needs to be made'.

**Sections 3.3.11 and 3.3.15, Sustainability Appraisal of Regional Spatial Strategies and Local Development Documents, DCLG, 2005.**

#### 3.1.1 Stage B involves six tasks:

- **B1** - Testing the DPD objectives against the SA framework
- **B2** - Developing the DPD options – carried out by the Council
- **B3** - Predicting the effects of the draft DPD
- **B4** - Evaluating the effects of the draft DPD
- **B5** - Considering ways of mitigating adverse effects and maximising beneficial effects
- **B6** - Proposing measures to monitor the significant effects of implementing the DPD

### 3.2 Testing the DPD Objectives (B1)

'It is important for the objectives of the DPD to be in accordance with sustainability principles. The objectives should be tested for compatibility with the SA objectives.'

**Section 3.3.4, Sustainability Appraisal of Regional Spatial Strategies and Local Development Documents, DCLG, 2005**

#### 3.2.1 The objectives of the DPD are not appraised in this Supplementary SA Report.

### 3.3 Developing the Options (B2)

The SEA Directive requires the environmental assessment to identify, describe and evaluate... 'the likely significant effects on the environment of a plan or programme' Annex II of the SEA Directive provides criteria which help determine the likely significance of an effect.

**SEA Directive, Article 5 & Annex II**

The Guidance states that the 'purpose of this task is to predict the social, environmental and economic effects of the options being considered in the DPD process'. Also, the Guidance states that 'having identified and described the likely effects of the DPD, an evaluation of their significance needs to be made'.

**Sections 3.3.11 and 3.3.15, Sustainability Appraisal of Regional Spatial Strategies and Local Development Documents, DCLG, 2005.**

3.3.1 In order to identify how best to meet the housing shortfall in the district, the Council tested to ensure that reasonable site options had been identified, and undertook a first stage assessment of the 35 sites identified to see if they passed fundamental planning considerations. Those that failed this assessment were rejected at this stage. The 16 sites that passed that first stage were then subject to full comparative assessment, and have been tested in this sustainability appraisal. Furthermore, the appraisal has also considered the relative merits of the North West Cambridge Area Action Plan site. The complete list of plan options is set out below:

- Parcel L2, Arbury Park
- Parcel Com 4, Arbury Park
- Parcel Q and H.R.C.C, Arbury Park
- North West Cambridge AAP
- Land between Huntingdon Road, Histon Road and the A14, North West Cambridge (Objection Site)
- Land between Huntingdon Road, Histon Road and the A14, North West Cambridge (South Cambs Revision)
- Land North of Barton Road
- Land north of Fen Road, Milton (Greengates Piggeries)
- Land south of Manor Park / Somerset Road
- Powell's garage, Woollards Lane
- The Ida Darwin Hospital
- Land between Teversham Road and Cow Lane

- Land West of Cambourne
- Land North of A428
- Bourn Airfield (1)
- Bourn Airfield (2)

## 3.4 Predicting and evaluation the effects (B3 & B4)

### Methodology

- 3.4.1 The ability to perform a comparative appraisal of alternative plan options is a key element of SA. Each of the options was assessed against each of the SA objectives, with the aim of highlighting the sustainability strengths and weaknesses of each location, and therefore aiding the process of deciding upon which should be selected as a 'preferred option'.
- 3.4.2 The options appraisal was a qualitative exercise based on professional judgement on the part of Scott Wilson taking into account the information gathered in the Scoping Report. For each option, the significance of effects in terms of each SA objective was evaluated by means of a commentary and also given a broad categorisation using the scoring system as shown in **Table 3.1**.

*Table 3.1: Appraisal scoring definitions<sup>11</sup>*

++	Option will result in a positive impact on the SA Objective
+	The impact on the SA Objective is dependant on implementation, but if there were to be an impact it would most likely be positive.
0	Neutral or negligible effect
-	The impact on the SA Objective is dependant on implementation, but if there were to be an impact it would most likely be negative.
--	Option will result in a negative impact on the SA Objective
?	The relationship between the Option and the SA Objective is unknown, or there is not enough information to make an assessment.

- 3.4.3 A summary of the sustainability performance of each option was prepared, which highlighted any trade-offs between SA objectives that it might be necessary to make should the site be developed, and also identified mitigation measures, assumptions and uncertainties.

<sup>11</sup> N.B. In some cases, differing symbols have been used to score the appraisal. Where this occurs the **first** symbol takes priority. For example an -? score will technically be a negative score, with some uncertainty, a ?- score will technically be a uncertain score but with the potential to be negative.

3.4.4 Although each option was appraised in a separate assessment table (i.e. somewhat in isolation) the evaluation of sustainability effects focused on identifying the *relative* sustainability of each option, although some consideration was also given to the possible ‘actual’ sustainability effects of developing a particular site. In order to aid this comparative assessment a summary table of the findings of the stand-alone appraisals has been prepared (see **Table 3.2**). The appraisal summaries and recommendations for each option can also be seen below, whilst the complete appraisal matrices can be found in Appendix I.

*Table 3.2: Options appraisal summary table*

Objective	Sub-objective	Parcel L2, Arbury Park	Parcel Com 4, Arbury Park	Parcel Q and H.R.C, Arbury Park	North West Cambridge AAP	Huntingdon /, Histon Road / A14, North West Cambridge	Huntingdon /, Histon Road / A14, North West Cambridge (South Cambs Revision)	land north of Barton Rd.	Land north of Fen Road, Milton (Greengates Piggeries)	Powell's garage	Ida Darwin Hospital	Teversham Road / Cow Lane	Land south of Manor Park / Somerset Road	Land West of Cambourne	Land North of A428	Bourne Airfield (1)	Bourne Airfield (2)
Land and water resources	Undeveloped land	+	+	+	-	-	-	-	?	+	+	-	-	-	-	?	?
	Non-renewable resources	0	0	0	+	0	0	0	0	0	0	0	0	0	0	0	0
	Water	0	0	0	0	0	0	0	0?	0	-?	-?	0	0?	0?	0?	0?
Biodiversity	Designated sites and species	+	+	+	?	?	?	-?	+	+	+	+	+	0?	-?	-?	-?
	Characteristic habitats and species	+	+	+	?	?	?	?-	?	+	+	?-	?	0-	?	-?	-?
	Access to wildlife and wild places	?	?	?	+	+	+	+	++	0	+	+	0	+	+	+	+



Landscape, townscape and archaeology	Historic interest	+	+	?	?-	?	?	+	?	?	+	?-	?	-?	-?	-?	-?
	Landscape and townscape	+	-?	+	--	--	-	-	-?	?	?	-?	-?	-	-	-	-
	Places, spaces and buildings	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Climate change and pollution	Greenhouse gasses and other pollutants	++	++	++	+	+	+	-?	?	+	+	?	?+	-	-	-	-
	Waste	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
	Vulnerability to climate change	+	+	+	?	?-	?+	--	-	+	?	?	+	?	?	?	?
Healthy communities	Health	-	-	-	?	-	?	-	-	0	0+	-	-?	0	0	?-	?-
	Crime, and fear of crime	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
	Publicly accessible open space	+	+	+	+	?+	?	?	?	0	+	0+	-	+	+	+	+
Inclusive communities	Services and facilities	+	+	+	++	+	+	?	-	+	?	+	+	-	-	?-	-?
	Inequalities	0	0	0	0	0	0	0	?-	0	0	0	0	0	0	0	0
	Housing	+	+	+	++	+	+	+	+	+	+	+	+	+	+	+	+
	Community	+	+	+	+	0	0	0	0?	0	0	0	0	?	?-	?-	?
Economic activity	+	+	+	+	+	+	0?	+	?	+	+	+	?	-	?	?	

**South Cambridgeshire District Council**  
 Sustainability Appraisal of the South Cambridgeshire Site Specific Policies DPD



	Investment in people, places and infrastructure	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
	Local economy	0	0	0	0	0	0	0	0	0	0+	0	0	+	0	0	0

- 3.4.5 SCDC have taken account of this process of SA as they have chosen their preferred options, i.e. those site options that they believe are best placed to make up the housing shortfall in the district. The findings of the Supplementary SA supported the findings of a separate appraisal of the site options against planning criteria undertaken by the Council. In fact, SA was also integrated into this planning appraisal, as the assessment criteria were developed so that they incorporated the SCDC SA objectives. Appendix II shows the links between the planning criteria and the SA objectives.
- 3.4.6 For each preferred option SCDC have drafted a policy that aims to guide future development at that location, recognising the local constraints and opportunities. The policies prepared for each of the preferred options have also undergone a process of SA. Rather than a comparative appraisal, the aim was to appraise each policy against the SA framework so as to determine the actual effects of implementing that Policy.
- 3.4.7 Again, appraisal summaries have been prepared that identify where the policies perform well (i.e. where they take account of locational constraints / opportunities to effectively mitigate negative effects of development / make the most of sustainability opportunities resulting from development). There is also a focus on identifying the significant residual sustainability effects that will remain, assuming that the Policy is fully implemented. Where possible recommendations have been made as to how residual effects might be mitigated / enhanced. However, it is recognised that many negative residual effects are currently very uncertain, and so as well as making further recommendations for Policy changes, detailed ideas have been set out with regard to what effects the Council may wish to monitor. Monitoring proposals are set out in Section 3.8.

### Options appraisal summary findings and recommendations

- 3.4.8 Set out below are the summaries and recommendations from the options appraisals. **Tables 3.3 – 3.18** show summaries of the options appraisals that were undertaken. These recommendations were made available to the Council to aid them as they went through the process of deciding on preferred options. Matrices detailing the appraisal of each option against each of the SA objectives can be found in Appendix I.

*Table 3.3: Parcel L2, Arbury Park*

#### Summary

There is little evidence that this proposed development site differs substantially from the other developments proposed at Arbury Park in terms of strategic sustainability considerations.

It forms part of a wider allocation carried forward from the Local Plan 2004 and included in the Submission Draft Site Specific Policies DPD and therefore subject to Sustainability Appraisal in the Final Sustainability Report 2006. The proposals are for different uses on certain parcels resulting in increase over 900 dwellings approved.

This wider Arbury Park site is already allocated for development and under construction. As such studies have already established that most environmental constraints are not a major consideration. The particular issues to be considered in the context of a change to residential

use relate to nearby sources of noise and air pollution. The site is nearby to the A14, which is due to be upgraded, including adding an additional lane, and is within a designated Air Quality Management Area (AQMA). It will be particularly important to ensure that residential development at this location would not be introducing sensitive receptors (residents) to an area where their health would be at risk.

It will form part of the urban area of Cambridge and is in a sustainable location in terms of ensuring good accessibility and reducing car dependency. The site would have good access to local services and facilities in the wider Arbury Park development and employment in the nearby Science Park, as well as by good public transport provision served by a number of routes, including the guided busway.

**Recommendations**

If this location is to go forward for further consideration it will be important to:

1. Ensure that that there will be no risk to health and well-being as a result of exposure to noise and air pollution.

*Table 3.4: Parcel Com 4, Arbury Park*

**Summary**

There is very little evidence that this proposed development site differs substantially from the other developments proposed at Arbury Park in terms of strategic sustainability considerations, although noise pollution is perhaps a marginally greater (even more significant) constraint at this site.

It forms part of a wider allocation carried forward from the Local Plan 2004 and included in the Submission Draft Site Specific Policies DPD and therefore subject to Sustainability Appraisal in the Final Sustainability Report 2006. The proposals are for different uses on certain parcels resulting in increase over 900 dwellings approved.

This wider Arbury Park site is already allocated for development and under construction. As such studies have already established that most environmental constraints are not a major consideration. The only exceptions relate to nearby sources of noise and air pollution. The site is nearby to the A14, which is due to be expanded, and is within a designated Air Quality Management Area (AQMA). It will be particularly important to ensure that residential development at this location would not be introducing sensitive receptors (residents) to an area where their health would be at risk.

It will form part of the urban area of Cambridge and is in a sustainable location in terms of ensuring good accessibility and reducing car dependency. The site would have good access to local services and facilities in the wider Arbury Park development and employment in the nearby Science Park, as well as by good public transport provision served by a number of routes, including the guided busway.

**Recommendations**

If this location is to go forward for further consideration it will be important to:

1. Ensure that that there will be no risk to health and well-being as a result of exposure to noise and air pollution.

*Table 3.5: Parcel Q and H.R.C.C, Arbury Park*

<p><b>Summary</b></p> <p>There is very little evidence that this proposed development site differs substantially from the other developments proposed at Arbury Park in terms of strategic sustainability considerations. Compared to the other two proposals, this site is slightly further from the major employment development at the Science Park and is perhaps slightly less constrained in terms of noise and air pollution, although the closer proximity to A14 junction and heavy traffic levels on Cambridge Road will be significant.</p> <p>This wider Arbury Park site is already allocated for development and under construction. As such studies have already established that most environmental constraints are not a major consideration. The only exceptions relate to nearby sources of noise and air pollution. The site is nearby to the A14, which is due to be expanded, and is within a designated Air Quality Management Area (AQMA). It will be particularly important to ensure that residential development at this location would not be introducing sensitive receptors (residents) to an area where their health would be at risk.</p> <p>It will form part of the urban area of Cambridge and is in a sustainable location in terms of ensuring good accessibility and reducing car dependency. The site would have good access to local services and facilities in the wider Arbury Park development and employment in the nearby Science Park, as well as by good public transport provision served by a number of routes, including the guided busway.</p> <p><b>Recommendations</b></p> <p>If this location is to go forward for further consideration it will be important to:</p> <ol style="list-style-type: none"><li>1. Ensure that that there will be no risk to health and well-being as a result of exposure to noise and air pollution.</li></ol>
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*Table 3.6: North West Cambridge AAP*

<p><b>Summary</b></p> <p>This is a greenfield site and there are a range of important considerations in terms of the ecological and landscape value of the land at this location. In terms of landscape, the setting of Cambridge will certainly suffer somewhat as a result of development here. Furthermore, there is a need to ensure no increase in known flood risk downstream.</p> <p>The site has potential to create a sustainable urban extension to Cambridge in terms ensuring good access to services and facilities and promoting sustainable modes of transport. This is a large site and a mixed use development is proposed with new local services and community facilities to serve the development.</p> <p>The site would also have good access to a wide variety of employment opportunities, consistent with the strategy to provide more homes focused on Cambridge to improve the jobs workers balance. There are also considerable sustainability benefits to consider relating to the fact that the development sets out to meet a specific housing need (that of university workers).</p> <p><b>Recommendations</b></p> <p>If this location is to go forward for further consideration it will be important to:</p> <ol style="list-style-type: none"><li>1. Ensure that the biodiversity value of the site and the surrounding landscape is not significantly reduced.</li></ol>
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2. Ensure that landscape effects are mitigated as far as possible.
3. Ensure that flood risk downstream does not increase.

*Table 3.7: Land between Huntingdon Road, Histon Road and the A14, North West Cambridge (Objector's site)*

<p><b>Summary</b></p> <p>This is a greenfield site and there are a range of important considerations in terms of the ecological and landscape value of the land at this location. The loss of Green Belt is an important consideration but not a fundamental constraint to development at this site. There are no other absolute constraints, although there is a degree of flood risk that must be mitigated and the potential to worsen existing poor air quality is an important consideration. There is also a need to ensure no increase in known flood risk downstream.</p> <p>The site would also have good access to a wide variety of employment opportunities, consistent with the strategy to provide more homes focused on Cambridge to improve the jobs workers balance. The site has potential to create a sustainable urban extension to Cambridge in terms of ensuring good access to services and facilities and promoting sustainable modes of transport. The site has potential to form part of a larger sustainable urban extension to Cambridge together with land already allocated in Cambridge City. The adjacent development site would bring forward significant improvements to both services and facilities and public transport close to the site and could be further enhanced either by additional services and facilities located within this site or by financial contributions brought forward as a result of development at this site.</p> <p><b>Recommendations</b></p> <p>If this location is to go forward for further consideration it will be important to:</p> <ol style="list-style-type: none"><li>1. Ensure that the biodiversity value of the site and the surrounding landscape is not significantly reduced.</li><li>2. Mitigate landscape impacts through the scale and footprint of any development and the landscaping and design of the development.</li><li>3. Ensure that flood risk does not increase.</li><li>4. Ensure that air poor air quality does not impact on health</li></ol>
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*Table 3.8: Land between Huntingdon Road, Histon Road and the A14, North West Cambridge (South Cambs Revision)*

<p><b>Summary</b></p> <p>This is a greenfield site and there are a range of important considerations in terms of the ecological and landscape value of the land at this location. The loss of Green Belt is an important consideration but not a fundamental constraint to development at this site. There are no other absolute constraints, although there is a degree of flood risk that must be mitigated and the potential to worsen existing poor air quality is an important consideration. This revised site footprint should significantly reduce the potential for poor air quality to impact on health. There is also a need to ensure no increase in known flood risk downstream.</p> <p>The site would also have good access to a wide variety of employment opportunities, consistent with the strategy to provide more homes focused on Cambridge to improve the jobs</p>
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workers balance. The site has potential to create a sustainable urban extension to Cambridge in terms of ensuring good access to services and facilities and promoting sustainable modes of transport. The site has potential to form part of a larger sustainable urban extension to Cambridge together with land already allocated in Cambridge City. The adjacent development site would bring forward significant improvements to both services and facilities and public transport close to the site and could be further enhanced either by additional services and facilities located within this site or by financial contributions brought forward as a result of development at this site.

### Recommendations

If this location is to go forward for further consideration it will be important to:

1. Ensure that the biodiversity value of the site and the surrounding landscape is not significantly reduced.
2. Mitigate landscape impacts through the scale and footprint of any development and the landscaping and design of the development.
3. Ensure that flood risk does not increase.

*Table 3.9: Land north of Barton Rd.*

### Summary

This is a greenfield site with some ecological and landscape constraints to consider. Whilst the site is small in size, development would nevertheless have an impact on this important part of the Green Belt setting of the City. Air quality could be a significant issue as a result of the proximity of two AQMAs and the likelihood of increasing levels of traffic on already congested roads. Flood risk is also a major constraint at this site.

The site has less potential to create a sustainable urban extension to Cambridge, in terms of ensuring good access to services and facilities and promoting sustainable modes of transport, than is the case for other sites on the edge of the City. This is primarily because of question marks over the potential to make public transport an attractive option as a mode of transport. Given the location of the site close to the M11, travel to sites such as the Park & Ride sites, Addenbrookes and the Science Park would be likely to be more attractive to be made by car. However, adjacent to the site is an off-road cycleway which leads into the city centre and the city cycle network.

### Recommendations

If this location is to go forward for further consideration it will be important to:

1. Ensure that adequate public transport can be accessed easily from the site
2. Ensure that biodiversity constraints are given adequate consideration
3. Ensure that flood risk on and off-site is minimised
4. Ensure that air poor air quality does not impact on health

*Table 3.10: Land north of Fen Road, Milton (Greengates Piggeries)*

**Summary**

This is a site with some areas of previously developed land, but which is mainly open land of low agricultural value. The site is in close proximity to the River Cam ecological corridor and may have some biodiversity value associated with this or as a result of other habitats and species present of local importance. The site has particularly good access to biodiversity rich greenspace, including a long-distance footpath along the River Cam. However, related to the proximity to the Cam, there is a degree of local flood-risk. Development could avoid the land at greatest risk, but a more detailed Flood Risk Assessment would be important. In terms of landscape, residential development of this site would change the character of this area, which retains something of a rural character. There are also constraints as a result of noise pollution from the nearby mainline railway.

Location of site on the edge of Cambridge would provide good access to a wide variety of employment opportunities, consistent with the strategy to provide more homes focused on Cambridge to improve the jobs:workers balance. There is a good local bus service into the city centre, but the nearest bus-stop for this service is located 1km away. Cycling and walking should be fairly attractive options for accessing employment and the city centre, although there will be a need to cross the level-crossing. However, local services and facilities are located a considerable distance away in Chesterton, and would not be easily accessible by walking.

The site is currently designated in Local Plan 2004 saved policy CNF6, as an area where permission may be granted for private gypsy sites to meet local need. This policy will be reviewed by the Gypsy and Traveller DPD which is currently in preparation.

**Recommendations**

If this location is to go forward for further consideration it will be important to:

1. Demonstrate that flood risk can be over-come through a detailed assessment.
2. Ensure that noise and odour pollution can be adequately avoided or mitigated.
3. Ensure that an opportunity to meet Gypsy and traveller housing need is not lost without due consideration

*Table 3.11: Land south of Manor Park / Somerset Road (Histon)*

**Summary**

This is a greenfield development, but on land that is currently underused, rather than being used for agriculture. However, there is an area of mature woodland onsite and the site is currently used informally for recreational purposes. The open nature (relating well to the countryside) and perceived recreational use means that development of this site would have an adverse impact on the character of this part of the village. Furthermore, the site is currently allocated to be 'developed' as formal open space in the future, and this lost opportunity is a significant consideration.

There is a good bus service and access to a high quality cycle network, although the bus stops and other services and facilities in Histon are located a considerable walk away. Therefore, it is unclear whether the location is particularly suited to reducing car-dependency.

The compatibility of residential development with the adjoining land uses, particularly with



regard to noise and odour, would need to be investigated, and issues may be difficult to overcome.

**Recommendations**

If this location is to go forward for further consideration it will be important to:

1. It will be important that further studies are undertaken to ascertain the true value of the grassland and woodland habitats
2. Ensure that any residual noise and odour constraints can be over-come by mitigation.

*Table 3.12: Powell's garage, Woollards Lane*

<p><b>Summary</b></p> <p>This is a brownfield site with no obvious biodiversity constraints. However, there are important heritage and townscape considerations as a result of the site being located within a conservation area.</p> <p>There is limited potential for additional facilities on this relatively small village centre site. However, since it is located in the village centre people would benefit by access to existing services and facilities which are located only a short walk distance from the site. In addition Cambridge City Centre is relatively easily accessible by a regular bus service. Major employment locations at Cambridge City Centre and Addenbroke's are in relatively close proximity. However the development would result in the loss of a small existing commercial site.</p> <p><b>Recommendations</b></p> <p>If this location is to go forward for further consideration it will be important to:</p> <ol style="list-style-type: none"><li>1. If this location is to go forward for further consideration it will be important to:</li><li>2. Demonstrate that the impact on the Conservation Area and the historic character of this part of the village could be addressed through an appropriate design.</li></ol>
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*Table 3.13: Ida Darwin Hospital*

<p><b>Summary</b></p> <p>This is a brownfield development at a location with no major biodiversity constraints. However, the Fulbourn Hospital site is designated as a Conservation Area and is a well wooded and attractive landscape. The site is in the Green Belt, but it is likely that the Green Belt function can be retained. The main environmental constraint is the sensitivity of the underlying aquifer, which could potentially be put at risk by inappropriate development. There is potential to make use of the existing landscaped areas and create publicly accessible open space, and there is also biodiversity rich countryside to the East of Fulbourn.</p> <p>The site is located relatively close to Cambridge and employment sites and offers the potential for sustainable travel by both bus and cycle, but is likely to still have a relatively high modal share for the private car unless journey times into the city by public transport can be reduced. The site has reasonable accessibility on foot to local services and facilities being around 15 minutes away.</p>
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**Recommendations**

If this location is to go forward for further consideration it will be important to:

1. Ensure groundwater recharge is not affected and that there is no risk of pollution to this important aquifer.
2. Ensure that noise issues area assessed and mitigated.
3. Ensure development is sympathetic to the adjacent Conservation Areas and the SAM as well as the setting of, Cambridge and Fulbourn more generally.
4. Seek to improve access to high quality public transport

*Table 3.14: Land between Teversham Road and Cow Lane*

**Summary**

There are important landscape and townscape considerations. Development would extend the built up area of the village north, towards the railway line. Development of part of the site, on the field currently fronting Teversham Road, would have minimal impact as the field is relatively well contained and has little impact on the character of the Conservation Area. However, development of the larger field to the rear of the Poor Well *would* have an impact on the character of the Conservation Area.

Furthermore, there are serious concerns regarding noise pollution from adjacent land-uses and the site is located in the Source Protection Zone above a sensitive aquifer, which could potentially be put at risk by inappropriate development.

The site is located relatively close to Cambridge and other employment sites and offers the potential for sustainable travel by both bus and cycle, but is likely to still have a relatively high modal share for the private car unless journey times into the city by public transport can be reduced. The site has reasonable accessibility on foot to local services and facilities being around 15 minutes away.

**Recommendations**

If this location is to go forward for further consideration it will be important to:

1. Ensure groundwater recharge is not affected and that there is no risk of pollution to this important aquifer.
2. Ensure that development is sensitive to the setting of the conservation area
3. Seek to improve access to high quality public transport

*Table 3.15: Land West of Cambourne*

**Summary**

This is a greenfield site on good quality agricultural land, albeit land without any major environmental constraints to development. The land is, however, important in terms of the landscape setting of Cambourne. Furthermore, the archaeological potential of the site is a constraint to be addressed.

Cambourne is the least sustainable of the Rural Centres in terms of location relative to Cambridge and quality of public transport. Reliance on the private car to access higher order services and facilities as well as major employment locations would most likely be relatively

high, and there are concerns about the potential to bring forward lower order services and facilities commensurate with the level of development within walking / cycling distance. The location of this site does, however, mean that there is good access to open and biodiversity rich countryside as well as other open space and play areas.

#### **Recommendations**

If this location is to go forward for further consideration it will be important to:

4. Consider foul water drainage constraints.
5. Consider the potential for impacts on the SSSI.
6. Incorporate existing biodiversity features into the masterplan in such a way that allows them to retain their functional value.
7. Establish the off-site impacts of increased surface water run-off at this location.
8. Ensure that a good range of local services and facilities are accessible within walking distance.
9. Consider how effects on the community / sense of place in Cambourne might be mitigated.
10. Give consideration to the high archaeological potential of the site.

*Table 3.16: Land North of A428*

#### **Summary**

This is a greenfield site on good quality agricultural land, with significant landscape and ecological constraints.

Cambourne is the least sustainable of the Rural Centres in terms of location relative to Cambridge and quality of public transport. Reliance on the private car to access higher order services and facilities as well as major employment locations would most likely be relatively high, and there are concerns about the potential to bring forward lower order services and facilities commensurate with the level of development within walking / cycling distance. The location of this site does, however, mean that there is good access to open and biodiversity rich countryside as well as other open space and play areas.

Development north of the A428 would not relate well to the existing form of Cambourne and would not provide a sustainable extension to the village in the sense that it is unlikely that walking or cycling will be an attractive form of movement between the two centres. There are a number of questions regarding whether provision of a new centre will compliment the services and facilities available at Cambourne. The location of this development does, however, mean that there is good access to open and biodiversity rich countryside.

#### **Recommendations**

If this location is to go forward for further consideration it will be important to:

11. Consider foul water drainage constraints.
12. Consider the potential for impacts on the SSSI.
13. Incorporate existing biodiversity features into the masterplan in such a way

- that allows them to retain their functional value.
14. Establish the off-site impacts of increased surface water run-off at this location.
  15. Ensure that a good range of local services and facilities are accessible within walking distance.
  16. Consider effects on the sustainability of the existing village centre at Cambourne
  17. Consider how effects on the community / sense of place in Cambourne might be mitigated.
  18. Give consideration to the high archaeological potential of the site.

*Table 3.17: Bourne Airfield (1)*

<p><b>Summary</b></p> <p>This is a mixed greenfield and brownfield development on a site that includes some ecological interest on-site as well as being in close proximity to important sites for biodiversity and is also important in terms of visual landscape value. Furthermore, the archaeological potential of the site is a constraint to be addressed, but is unlikely to be an absolute constraint.</p> <p>Cambourne is the least sustainable of the Rural Centres in terms of location relative to Cambridge and quality of public transport. Reliance on the private car to access services and facilities as well as employment would most likely be relatively high, despite the fact that the proposals to develop a new local centre.</p> <p>Development on Bourn Airfield would not relate well to the existing form of Cambourne and would not provide a sustainable extension to the village in the sense that it is questionable whether walking or cycling will be an attractive form of movement between the two centres. There are a number of questions regarding whether provision of a new centre will compliment the services and facilities available at Cambourne. The location of this development does, however, mean that there is good access to open and biodiversity rich countryside as well as other open space.</p> <p><b>Recommendations</b></p> <p>If this location is to go forward for further consideration it will be important to:</p> <ol style="list-style-type: none"><li>19. Consider foul water drainage constraints.</li><li>20. Consider the full range of potential effects that could impact on the ecological health of the SSSI and incorporate existing biodiversity features into the masterplan in such a way that allows them to retain their functional value.</li><li>21. Clarify the nature of the impact to the County Wildlife Site</li><li>22. Give consideration to the high archaeological potential of the site.</li><li>23. Establish the off-site impacts of increased surface water run-off at this location.</li><li>24. Consider how residents might access services, facilities and employment</li></ol>
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- easily by means other than the private car.
25. Consider effects on the sustainability of the existing village centre at Cambourne
  26. Consider how effects on the community / sense of place in Cambourne might be mitigated.

*Table 3.18: Bourne Airfield (2)*

**Summary**

This is a mixed greenfield and brownfield development on a site that includes some ecological interest on-site as well as being in close proximity to important sites for biodiversity and is also important in terms of visual landscape value. Furthermore, the archaeological potential of the site is a constraint to be addressed, but is unlikely to be an absolute constraint. The fact that this proposal has a reduced development footprint may mean that some effects are less significant relative to the Bourn Airfield (1) proposal.

Cambourne is the least sustainable of the Rural Centres in terms of location relative to Cambridge and quality of public transport. Reliance on the private car to access services and facilities as well as employment would most likely be relatively high, despite the fact that the proposals to develop a new local centre.

Development on Bourn Airfield would not relate well to the existing form of Cambourne and would not provide a sustainable extension to the village in the sense that it is questionable whether walking or cycling will be an attractive form of movement between the two centres. It is currently uncertain whether the development would be of a scale to deliver its own centre. The location of this development does, however, mean that there is good access to open and biodiversity rich countryside as well as other open space and play areas.

**Recommendations**

If this location is to go forward for further consideration it will be important to:

27. Consider foul water drainage constraints.
28. Consider the full range of potential effects that could impact on the ecological health of the SSSI and incorporate existing biodiversity features into the masterplan in such a way that allows them to retain their functional value.
29. Clarify the nature of the impact to the County Wildlife Site
30. Give consideration to the high archaeological potential of the site.
31. Establish the off-site impacts of increased surface water run-off at this location.
32. Consider how residents might access services, facilities and employment easily by means other than the private car.
33. Consider how effects on the community / sense of place in Cambourne might be mitigated.

## Preferred options appraisal summary findings and recommendations

- 3.4.9 Set out below (**Tables 3.19 – 3.22**) are the summaries and recommendations from the appraisals of the preferred options. The recommendations aim to mitigate or enhance the absolute sustainability effects of implementing the Plan, and therefore it is important that the Council makes a statement in response to each. The Council's responses to these key recommendations can be viewed in Appendix IV.
- 3.4.10 There are few recommendations because many absolute effects will be mitigated by plan policies and so it is difficult to predict residual effects with any certainty. As a result, detailed suggestions have been made for monitoring in Section 3.8.

*Table 3.19: Land between Huntingdon Road, Histon Road and the A14, North West Cambridge (South Cambs revision)*

### Summary:

This is a detailed policy which focuses on ensuring that a number of the key sustainability issues presented by major development at this location are addressed. In particular, the Policy focuses on mitigating landscape impacts associated with the loss of an important area of Green Belt and ensuring water efficiency and minimising flood-risk. Traffic congestion and air / noise pollution associated with the A14 is also a constraint that is given careful consideration by the Policy. The Policy also seeks to make the most of locational opportunities by promoting an integrated approach to delivering biodiversity and high quality landscapes alongside the provision of open space and countryside access. This is a location that has the potential to allow for sustainable transport patterns amongst residents. The Policy seeks to build on this potential by stating what will be delivered on the ground in terms of public transport and high quality cycling and walking routes.

Nonetheless, there will be some residual negative sustainability impacts that cannot be wholly mitigated by the Policy. Foremost is the issue regarding the loss of productive agricultural land in the Green Belt and the impacts of this on the landscape setting of Cambridge. Effects may be particularly severe during the construction phase, reducing over time as a result of landscaping. Similarly, development at this location will put strain on water resources and potentially lead to negative effects in terms of air quality and noise objectives, but effects will be mitigated to some degree by the requirements of the Policy.

### Recommendations:

1. Clarify why a 'balance may need to be struck' in terms of the provision of affordable housing.
2. Demonstrate that services and facilities will be adequately accessible for all members of society, including the elderly and disabled, from the day that people move into the new development.
3. Add clarity to this requirement for exemplar developments

*Table 3.20: Powell's Garage, Great Shelford*

**Summary:**

This is a small scale redevelopment within the village of Great Shelford. The Policy aims to ensure that the two major locational constraints will be adequately addressed as part of future development, namely the integrity of the conservation area and the potential issue of land contamination. It is possible that there may be some negative residual effects on the integrity of the conservation area, although good design will go a long way to mitigating effects and could even result in positive effects in terms of townscape. A high density of development is proposed to make the most of the fact that this is a brownfield location with good access to local services and facilities as well as public transport. Because of the relatively modest size of any potential development, the Policy is silent on all other issues, instead relying on Development Control Policies DPD policies to ensure that potential sustainability issues are addressed.

**Recommendations:**

None

*Table 3.21: Ida Darwin Hospital, Fulbourn*

**Summary:**

This Policy has a focus on addressing the key locational constraint, which is the presence of the site within the Cambridge Green Belt. Development at this site, as guided by this Policy can be considered as a re-configuration of the Major Developed Site in the Green Belt, as a green wedge will be created where there is currently built development, providing compensatory open space with a countryside character. The Policy promotes the opportunity for the green wedge to lead to biodiversity gain and improved access to green space for the residents of Fulbourn village. The Policy refers to the requirement for a biodiversity strategy, and the supporting text highlights that the reversion of land to open countryside presents opportunities for biodiversity enhancement. The Policy and/or supporting text also refer to other key constraints to development at this location, namely land contamination, flood risk, the designated conservation area and noise and vibration from the nearby railway. There area unlikely to be any significant residual negative sustainability effects associated with these constraints, assuming that the Policy and other Development Control Policies DPD policies are fully implemented.

Even though this is not a small development, there are a number of other issues that are not elaborated on in the Policy, but rather Development Control Policies DPD policies will be utilised to ensure that there are no negative sustainability impacts and that sustainability benefits are maximised.

**Recommendations:**

None

*Table 3.22: Arbury Park, Cambridge*

**Summary:**

This is a Policy to guide a large mixed-use development. The Policy promotes making the most of opportunities for sustainable transport and ecological enhancement. It takes a range of important constraints into detailed account, particularly the mitigation of noise and emissions from the A14, the need to protect the landscape setting Cambridge, the need to minimise traffic congestion and the need to protect the Arbury Camp site of archaeological interest. There are significant local constraints in terms of traffic congestion and associated noise and air quality impacts. Implementation of the Policy will go some distance to mitigating effects, and it is likely

that the A14 improvements will improve the situation, but it will be important to monitor to ensure no residual effects, particularly in terms of health the health of residents. Even though this is a large development, there are a number of other issues that are not elaborated on in the Policy, but rather Development Control Policies DPD policy only will be utilised to ensure that there are no negative sustainability impacts and that sustainability benefits can be maximised.

**Recommendations:**  
None

## 3.5 Cumulative Impacts

- 3.5.1 It is a requirement of the SEA Directive (Annex 1 (f)) to assess the cumulative impacts of the plan<sup>12</sup>. In order to assess the cumulative impacts, Scott Wilson looked at the impacts of the preferred sites across the SA objectives and in combination with the cumulative effects assessment of the submitted Final SA Report<sup>13</sup>.
- 3.5.2 The scope of impacts was then increased to look at the possible impacts that could occur in the context of Cambridge. **Table 3.23** illustrates the findings of the cumulative impact assessment. It will be useful to also refer to **Figure 3.1** which provides an illustration of the Preferred Sites and Environmental Constraints.
- 3.5.3 The cumulative effects identified in the SA of the Site Specific Polices include:
- the piecemeal land-take that results from development occurring within designated areas.
  - growing pressure inside the Village Frameworks due to the repeated allocating of small sites.
  - corresponding land-take and associated pressures on the surrounding land to absorb the possible expansion of the existing Village Frameworks.
- 3.5.4 It should also be noted that for this assessment, *“insofar as mitigation is concerned, there is limited scope. This is due to the development pressure through UK Policy and an expanding population.”*

<sup>12</sup> This includes synergistic and secondary effects under this definition.

<sup>13</sup> See:

[http://www.scambs.gov.uk/admin/documents/retrieve.asp?pk\\_document=3611&bcsi\\_scan\\_F6892CABA15785B4=Rsb3GAmAQqdm3KL6/geQCSAAAADOZUYI:1](http://www.scambs.gov.uk/admin/documents/retrieve.asp?pk_document=3611&bcsi_scan_F6892CABA15785B4=Rsb3GAmAQqdm3KL6/geQCSAAAADOZUYI:1)



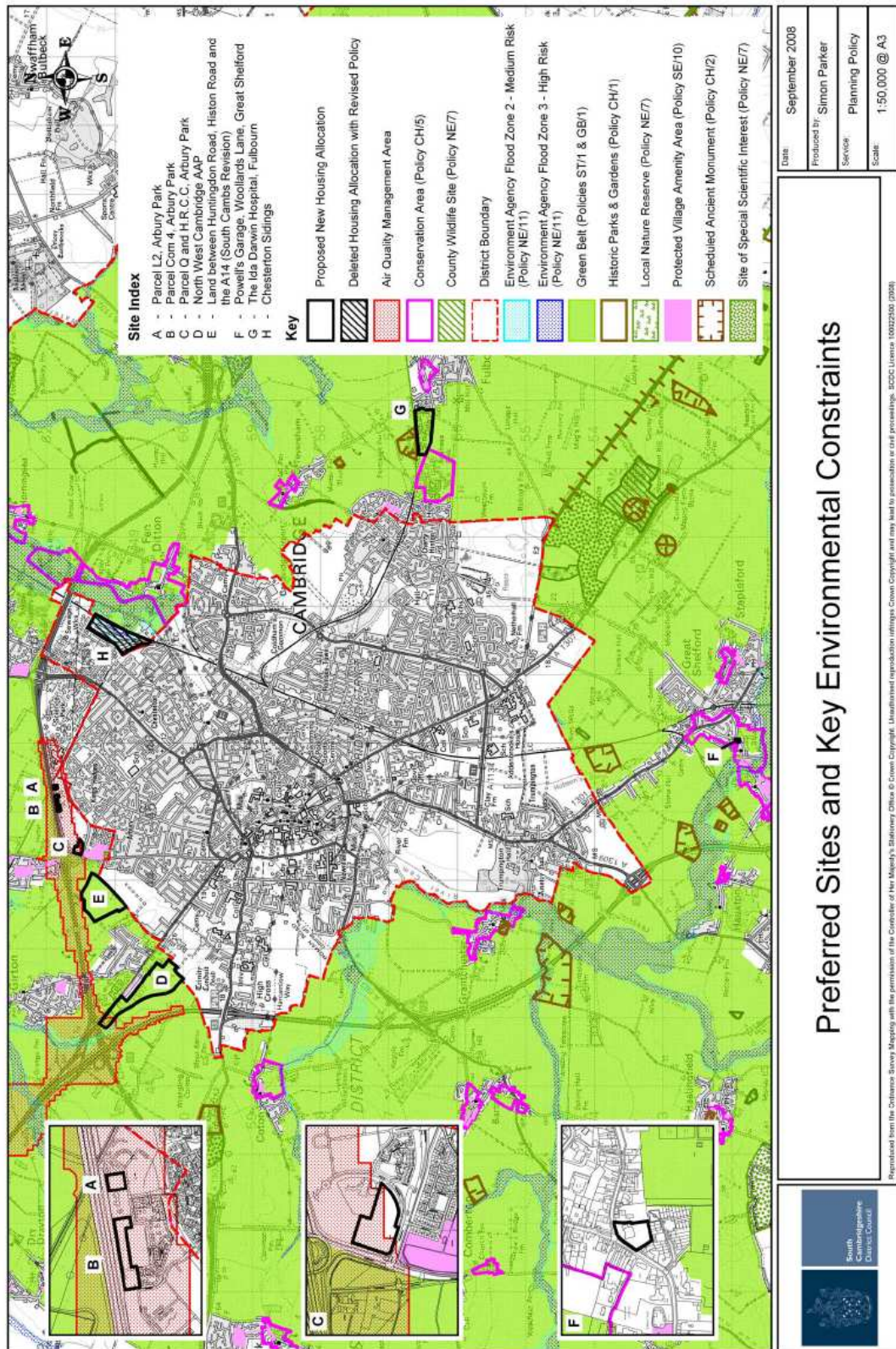


Figure 3.1: Preferred sites and environmental constraints

*Table 3.23: Cumulative effects*

Cumulative Effect	Mitigation / recommendation
<b>Cambridge Scale</b>	
<p>The addition of more sites on the fringe of Cambridge (Sites E,D and G are all in the Green Belt), will exacerbate existing pressure on the Green Belt and corresponding green wedges, threatening the character of this compact city and creating increased pressure on the landscape character of the area.</p>	<p>None proposed</p>
<p>With regard to biodiversity, by a large the sites in combination will have an unknown cumulative effect. However there is a concern that by impinging on the Green Belt and through sprawl that development may close in or place pressure upon sites designated for nature conservation. In addition the increased attractiveness of the area and the higher number dwellings in the area may result in increased recreation or other pressure on biodiversity sites.</p>	<p>The emphasis for recreation should be on green spaces and sites within the proposed sites.</p>
<p>The increased number of sites on the fringe of Cambridge may create cumulative landscape impacts. In particular sites C, E and D lie in proximity to Protected Village Amenity Areas.</p>	<p>Sensitive design of fringe settlements.</p>
<p>There will be cumulative impacts through the construction of new dwellings (embodied energy of materials, freight emissions etc) which will add to those identified in the SA of the SSP DPD. However, over time, energy efficiency standards may offset this <i>provided</i> transport policies create a situation where emissions are reduced. The proposal set out in the revised DPD for a railway station and transport interchange at Chesterton Sidings will have a significant effect, particularly if considered cumulatively with other transport improvements.</p> <p>It is noticeable also that three of the preferred sites lie within AQMAs (A, B and C), and that these sites are clustered together in the North West. It will be important that air quality is monitored closely in these areas to ensure that significant</p>	<p>Possibility of combined Green Travel Plans for sites A, B and C to ensure that they don't contribute to poor air quality in the AQMA.</p>

decreases in air quality do not occur.	
By and large the benefits to health should be positive, with additional housing provision (and corresponding affordable housing) going towards meeting the identified housing shortfall	None identified.
The scattered approach to the sites that have come forward should ensure a fair level of accessibility to community facilities as the housing allocation does not concentrate on one area. This is provided that there is no overburden in areas in proximity to these sites. It should be noted that sites C, E and D lie in proximity to Protected Village Amenity Areas. These should be either safeguarded or improved dependant on predicted levels of use.	Safeguard Protected Village Amenity Areas near proposed sites.
The cumulative effect of the site allocation should ensure that the housing shortfall is met, thus creating a situation where workers can contribute to the wider economic development of South Cambridgeshire.	None proposed.

### 3.6 Summary of the effects of the plan

- 3.6.1 The following section discusses the likely sustainability effects of the implementation of the DPD. It considers the effects of bringing forward the preferred options in combination with the sites identified in the previous submission Site Specific Policies DPD. The discussion in this section is not exhaustive, and should not be read in isolation, but rather is intended to summarise some of the key sustainability benefits, dis-benefits and trade-offs identified through the SA.
- 3.6.2 There are clear socio-economic benefits at the Cambridge scale of further development at the preferred Objection Sites. Further development will go some way to meeting the demand for housing and affordable housing, and new housing for employees is important if the Cambridge economy is to continue to prosper. This SA Report has highlighted that delivering further growth at the preferred sites will result in only limited significant trade-offs in terms of other socio-economic SA objectives. It does not appear that existing communities will be significantly impacted by development at any of the sites (although there may be some potential for impacts during the construction phase of development).
- 3.6.3 However, the choice of several of the sites has raised questions as to whether there may be the potential for the health of new residents to suffer as a result of air and noise

pollution. Several of the sites also have the potential to impact on already significant levels of traffic congestion, particularly on the A14. However, the plan does seek to mitigate these potential effects, and so significant residual effects cannot be predicted with any certainty at this stage, but must instead be monitored closely where possible.

- 3.6.4 Development on this scale at the preferred Objection Sites will inevitably result in a range of trade-offs needing to be made in terms of important environmental SA objectives. Significant development will come forward within the Cambridge Green Belt and therefore there is the potential for local biodiversity baseline to be affected and for the landscape setting of Cambridge to be eroded. Plan policies do seek to mitigate these effects, and in the case of biodiversity in particular, if the issue remains an important consideration throughout implementation of the plan, then it is likely that significant residual effects can be in many ways avoided.
- 3.6.5 Another key impact is related to the fact that, almost no-matter how well designed houses are to ensure water efficiency, further development in and around Cambridge will put significant additional strain on the already stretched water resources of the East of England. The previously submitted Final SA Report also highlighted this effect, and so it is clear that bringing forward the preferred Objection Sites in addition to wider development will exacerbate pressure on the region's water resources.
- 3.6.6 Another residual environmental effect, the significance of which is somewhat uncertain, is the potential to increase flood-risk (primarily off-site by contributing to increased run-off, as none of the proposed sites are themselves at immediate risk of fluvial flooding). With all environmental effects (flood risk is only one example) it will be important to consider the potential for negative sustainability effects to act cumulatively.
- 3.6.7 Development at the proposed sites may be considered to have some key sustainability benefits. In particular, the preferred sites all perform well in terms of increasing the potential for residents to adopt more sustainable transport patterns. In this sense, and given that the business as usual scenario would be likely to involve people commuting longer distances into Cambridge, development at these locations is likely to allow for a decrease in the per capita carbon footprint of new residents. However, it will be important that this opportunity for significant sustainability benefits is capitalised upon through appropriate investment and other incentives to change unsustainable behaviour.
- 3.6.8 A final key sustainability consideration of the revised DPD relates to the proposal to delete policy SP/2 of the original plan. The effect of this change will be to lose an allocation of 600 dwellings in the North East of the City, but instead to safeguard the land for the development of a railway station and interchange facility. Transport interchanges of this type (this interchange will link with the Cambridge Guided Busway) will be important in order to achieve a City-wide renaissance in the way that people travel.

## 3.7 Monitoring (B5)

The SEA Directive states that ‘member States shall monitor the significant environmental effects of the implementation of plans and programmes’.

**Article 10, the SEA Directive**

‘Monitoring allows the actual significant effects of implementation of the DPD to be tested against those predicted in the SA’.

**Section 3.3.22, Sustainability Appraisal of Regional Spatial Strategies and Local Development Documents, DCLG, 2005**

- 3.7.1 The SEA Directive includes a specific requirement for monitoring the significant environmental effects of plans and programmes and the Environmental Report (incorporated within this report) should include a description of the measures envisaged for doing so.
- 3.7.2 SCDC developed a detailed monitoring framework at the Sustainability Appraisal scoping stage (see Appendix 6 of the Scoping Report), which suggests a number of suitable significant effects indicators to monitor changes in the sustainability baseline. The monitoring information is published annually through the Council's Annual Monitoring Reports. It will be important that SCDC ensure that their monitoring framework continues to evolve to take account of any new significant effects that might be suggested by this SA Report or any other. The monitoring recommendations presented in **Table 3.24** are not in the form of indicators for which data can easily be collected, and some of the suggestions are rather aspirational, but the Council may wish to take these recommendations into account as part of internal discussions as to the most appropriate significant effects indicators that can realistically be collected.

*Table 3.24: Sustainability objectives for which the effects of the plan might be monitored*

Sustainability objective (adapted from the SCDC LDF SA framework)	Location at which significant effects are most likely	Suggestions for potential monitoring
Use land efficiently	<ul style="list-style-type: none"> <li>Arbury Park</li> </ul>	Monitor whether development is coming forward at a suitably high density.
Reduction of greenhouse gases by reducing energy consumption?	<ul style="list-style-type: none"> <li>All / cumulative</li> </ul>	Monitor whether all buildings are being built to ambitious standards of sustainable design and construction
Increase the proportion of energy and other resources being met from renewable	<ul style="list-style-type: none"> <li>All / cumulative</li> </ul>	Monitor whether all buildings are being built to ambitious standards of sustainable design

sources		and construction (so that per capita domestic carbon emissions are reduced).
Reduce water consumption	<ul style="list-style-type: none"> <li>All / cumulative</li> </ul>	Monitor to ensure that ambitious reductions in per capita water consumption are achieved.
Conserve groundwater resources	<ul style="list-style-type: none"> <li>All / cumulative</li> </ul>	Monitor to ensure that ambitious reductions in per capita water consumption are achieved.
Protect or enhance sites, features of areas of historical, archaeological, or cultural interest	<ul style="list-style-type: none"> <li>Land between Huntingdon Road and A14, Cambridge</li> <li>Powell's Garage, Great Shelford</li> <li>Ida Darwin Hospital</li> </ul>	Monitor to ensure that sites, features of areas of historical, archaeological, or cultural interest and their settings are protected (especially during the construction phase of development).
Maintain and enhance the diversity and distinctiveness of landscape and townscape character	<ul style="list-style-type: none"> <li>Land between Huntingdon Road and A14, Cambridge</li> <li>Powell's Garage, Great Shelford</li> </ul>	Monitor to ensure that the diversity and distinctiveness of landscape and townscape character is protected, especially (during the construction phase of development) and not eroded over time.
Maintain and enhance the character of settlements	<ul style="list-style-type: none"> <li>Land between Huntingdon Road and A14, Cambridge</li> <li>Powell's Garage, Great Shelford</li> <li>Arbury Park</li> </ul>	Monitor to ensure that the character of settlements is maintained, (especially during the construction phase of development).
Improve the satisfaction of people with their neighbourhoods as places to live	<ul style="list-style-type: none"> <li>Land between Huntingdon Road and A14, Cambridge</li> </ul>	Monitor to ensure satisfaction of people with their neighbourhoods as places to live is maintained (especially during the construction phase of development), and where possible improved.
Reduce emissions of greenhouse gases	<ul style="list-style-type: none"> <li>Ida Darwin Hospital</li> </ul>	Monitor travel patterns (are people able to reduce their per capita carbon footprint?)
Improve air quality	<ul style="list-style-type: none"> <li>Land between Huntingdon Road and A14</li> </ul>	Monitor to ensure that the health of residents is not adversely impacted upon by poor air quality, and that air quality in

	<ul style="list-style-type: none"> <li>• Arbury Park</li> </ul>	AQMAs is not worsened as a result of development.
Reduce traffic volumes	<ul style="list-style-type: none"> <li>• Land between Huntingdon Road and A14</li> <li>• Arbury Park</li> </ul>	Monitor traffic congestion around sensitive areas.
Support travel by means other than the car	<ul style="list-style-type: none"> <li>• Ida Darwin Hospital</li> </ul>	Monitor to ensure that sustainable modes of transport are accessible for all. Monitor the uptake of sustainable travel choices (including walking and cycling) over the lifecycle of any development.
Reduce levels of noise or noise concerns	<ul style="list-style-type: none"> <li>• Land between Huntingdon Road and A14, Cambridge</li> <li>• Ida Darwin Hospital</li> <li>• Arbury Park</li> </ul>	Monitor to ensure that noise pollution is not adversely impacting upon health
Improve water quality including by reducing diffuse and point source water pollution	<ul style="list-style-type: none"> <li>• Ida Darwin Hospital</li> </ul>	Monitor to ensure that groundwater protection zones are not adversely impacted upon.
Improve the quality and range of services and facilities	<ul style="list-style-type: none"> <li>• Land between Huntingdon Road and A14, Cambridge</li> </ul>	Monitor the uptake of sustainable travel choices (including walking and cycling) over the lifecycle of any development.
Improve accessibility to key local services and facilities	<ul style="list-style-type: none"> <li>• Land between Huntingdon Road and A14, Cambridge</li> </ul>	Monitor the uptake of sustainable travel choices (including walking and cycling) over the lifecycle of any development.
Improve accessibility by means other than the car and improve the attractiveness of environmentally better modes including public transport, cycling and walking	<ul style="list-style-type: none"> <li>• Land between Huntingdon Road and A14, Cambridge</li> </ul>	Monitor the uptake of sustainable travel choices (including walking and cycling) over the lifecycle of any development.
Promote accessibility for all members of society, including the elderly and disabled	<ul style="list-style-type: none"> <li>• Land between Huntingdon Road and A14, Cambridge</li> </ul>	Monitor to ensure that services and facilities are easily accessible to all.

## 3.8 Difficulties encountered

The SEA Directive requires *'any difficulties (such as technical deficiencies or lack of know-how) encountered in compiling the required information'* be included in the report.

Annex 1h, the SEA Directive

- 3.8.1 Perhaps the greatest difficulty related to the lack of evidence at the sub-authority and site level that could be drawn upon to make judgements about the relative and actual effects of plan options and preferred options. However, this difficulty was overcome to some degree by ensuring that the planning criteria, for which evidence was being gathered to answer, were developed so that they would also identify performance in terms of the SA objectives.
- 3.8.2 The other key problem related to the difficulty of predicting and evaluating the actual, or residual effects of the preferred options. It was clear that many of the sustainability effects would be mitigated by the policies prepared by the Council, but residual effects would depend on how the policies are implemented on the ground, which in many ways is difficult to gauge with any certainty. Because of the difficulty in predicting mitigation measures it was seen as important to set out detailed, although perhaps rather aspirational, suggestions for monitoring that the Council may wish to consider if they are to ensure sustainable implementation of the plan.



## 4 What happens next?

### 4.1 Introduction

- 4.1.1 The draft DPD will be subject to public consultation. This SA Report will also be available for consultation, in order to allow stakeholders to gain a better understanding of the sustainability effects of the plan and therefore make a more informed consultation response.

### 4.2 SEA / SA Statement

Plan or programme proponents should ensure that, when a plan or programme is adopted, the Environmental Consultation Bodies and the public “are informed and the following items are made available to those so informed:

- (b) a statement summarising how environmental considerations have been integrated into the plan or programme...[including] the reasons for choosing the plan or programme as adopted, in the light of the other reasonable alternatives dealt with, and

(Annex 9(1)), the SEA Directive

- 4.2.1 Once a plan or programme has been adopted, the SEA Directive requires those responsible for preparing it – in this case the Council - to provide the public and the Consultation Bodies with information on how environmental considerations and consultation responses are reflected in the plan or programme and how its implementation will be monitored in the future (in accordance with planning regulations, this will be done at adoption).
- 4.2.2 In light of this requirement, the Council will prepare an **SEA / SA Statement** setting out the above information. This SEA / SA statement will include the councils response to the recommendations set out in this report as well as set out final proposals for monitoring.

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## Appendix I: Sustainability Objectives and links to Site Assessment Criteria


Sustainability Appraisal Objective		Relevant Site Assessment Criteria	Why?
<b>Land and Water Resources</b>			
1.1	Minimise the irreversible loss of undeveloped land and productive agricultural holdings	3n, 3o, d	The re-use of previously developed land (PDL) will minimise the loss of undeveloped land. Policy NE/17 requires the protection of high quality agricultural land (grades 1, 2 & 3a). Higher density developments will make more efficient use of available sites.
1.2	Reduce the use of non-renewable resources including energy sources	2a, d	The sustainability of the settlement; and the density of development will have an impact on the sustainability of the site, and therefore on levels of greenhouse gas emissions.
1.3	Limit water consumption to levels supportable by natural processes and storage systems	3q	The absolute effect of further development will be to put additional strain on already stretched water resources in the sub-region. The actual impact will be dependent on site-specific implementation of water conservation measures.  Drainage issues, particularly relating to waste water, may also be relevant.
<b>Biodiversity</b>			
2.1	Avoid damage to designated sites and protected species	1c, 1d, 3h, 3i, 3j, 3u	Relevant indicators address designated sites e.g. SSSIs, Local Nature Reserves, European Nature Conservation Sites, County Wildlife Sites and TPOs.
2.2	Maintain and enhance the range and viability of characteristic habitats and species	3u	Criteria explores the impact on habitats and species.
2.3	Improve opportunities for people to access and appreciate wildlife and wild places	3k, 3l, 4h	Public Rights of Way are important in allowing people to access and appreciate wild places. Some sites may also provide the potential to improve access to the countryside.

<b>Landscape, townscape and archaeology</b>			
3.1	Avoid damage to areas and sites designated for their historic interest, and protect their settings.	1e, 1f, 1g, 3e, 3m	Relevant indicators address impact on areas and sites designated for their historic interest e.g. Scheduled Monuments, Listed Buildings, Historic Parks & Gardens, Conservation Areas and non-statutory archaeological sites.
3.2	Maintain and enhance the diversity and distinctiveness of landscape and townscape character	f, 3e, 3f, 3g, 4b, 4d, 4e, 4f	Important Countryside Frontages, Protected Village Amenity Areas and Conservation Areas ensure protection of the landscape and townscape character. Site context (e.g. topography, views, impact on surroundings) is important in assessing the impact on the local character.
3.3	Create places, spaces and buildings that work well, wear well and look good		Effects will depend on the final masterplan for a site.
<b>Climate change and pollution</b>			
4.1	Reduce emissions of greenhouse gasses and other pollutants (including air, water, soil, noise, vibration and light)	2a, 2b, 2d, 2h, 2i, 2j, 2k, 2l, 2m, 3s, 3t	The sustainability of the settlement; the accessibility of key services, employment and good / high quality public transport; and the density of a development will have an impact on the sustainability of the site, and therefore on levels of greenhouse gas emissions and air quality.
4.2	Minimise waste production and support the recycling of waste products		At this stage there is no evidence to suggest how suitable development at this location will be for adopting sustainable waste management practices
4.3	Limit or reduce vulnerability to the effects of climate change (including flooding)	1b	The indicator considers flood risk, and is directly relevant.
<b>Healthy communities</b>			
5.1	Maintain and enhance human health	2a, 2b, 2d, 2k, 2l, 3t	The sustainability of the settlement; the accessibility of key services, employment and the density of a development will have an impact on the sustainability of the site, which in turn will have an impact on human health.

5.2	Reduce and prevent crime, and reduce the fear of crime		There is no evidence available to suggest whether this location will be particularly suited or unsuited to preventing crime or the fear of crime. Much will depend on the final masterplan.
5.3	Improve the quantity and quality of publicly accessible open space	4h	Sites will be required to make provision according to the Council's open space standards. There may be potential for some sites to enhance access to the countryside and public openspace.
<b>Inclusive communities</b>			
6.1	Improve the quality, range and accessibility of services and facilities (e.g. health, transport, education, training, leisure opportunities)	2b, 2d, 2e, 2f, 2i, 2g, 2h, 2j, 2l, 4c	Relevant indicators address whether a site is suitable for a mix of uses, and therefore whether the quality, range and accessibility of services and facilities can be improved. Accessibility to services criteria are also relevant.
6.2	Redress inequalities related to age, gender, disability, race, faith, location and income	2a, 2b, 2d, 2h, 2i, 2j, 2l	The sustainability of the settlement; the accessibility of key services, employment and good / high quality public transport; and the density of a development will have an impact on the sustainability of the site. More sustainable locations will help to address any inequalities related to age, gender, disability and income [i.e. less need for a car, makes travel easier for older people, disabled and low income].
6.3	Ensure all groups have access to decent, appropriate and affordable housing	d	It is likely that all sites would be required to make provision for affordable housing, in line with the Council's adopted policies. Scale of contribution will depend on the scale of the site.
6.4	Encourage and enable the active involvement of local people in community activities		There is no evidence available to suggest whether a location will be particularly suited or unsuited to active involvement of local people in community activities.
<b>Economic activity</b>			
7.1	Help people gain access to satisfying work appropriate to their skills, potential and place of residence	2b, 2f	Good accessibility to local employment will help people gain access to satisfying work.
7.2	Support appropriate investment in people, places, communications and	2d, 2g, 2l, 4c, 4i	Relevant indicators address potential improvements in services, facilities and infrastructure.

	other infrastructure		
7.3	Improve the efficiency, competitiveness, vitality and adaptability of the local economy		The general provision of residential development will contribute to the objective of improving the homes jobs balance of the area.

**KEY**

 No matched site assessment criteria.

## Appendix II: Options assessment matrices

### Assessment table for Parcel L2, Arbury Park.

SA Topic	SA Objective	Assessment findings
Land and water resources	1.1 Minimise the irreversible loss of undeveloped land and productive agricultural holdings	+? The site was formerly agricultural land (Grade 2), but has been cleared for development and forms part of a wider area with outline planning permission for development.
	1.2 Reduce the use of non-renewable resources including energy sources	0 There is no evidence to suggest whether this site is particularly suitable or unsuitable for sustainable design and construction and renewable energy technologies. Effects are expected to be dependent on the development of the masterplan.
	1.3 Limit water consumption to levels supportable by natural processes and storage systems	0 The absolute effect of further development will be to put additional strain on already stretched water resources in the sub-region, therefore possibly leading to lowered groundwater levels and impacts on natural processes. However, no particular constraints to this site in terms of water resources have been identified.
Biodiversity	2.1 Avoid damage to designated sites and protected species	+ Development at this location should not lead to significant impacts on protected species.
	2.2 Maintain and enhance the range and viability of characteristic habitats and species	+ Development at this location would not have any significant impacts on the range and viability of characteristic habitats and species.
	2.3 Improve opportunities for people to access and appreciate wildlife and wild places	? The nearest public right of way is Mere Way, starting around 200m to the east of the site, which passes through open countryside to Landbeach.
Landscape, townscape and archaeology	3.1 Avoid damage to areas and sites designated for their historic interest, and protect their settings	+ This location will avoid damage to areas and sites designated for their historic interest, and protect their settings
	3.2 Maintain and enhance the diversity and distinctiveness of landscape and townscape character	+ Development at this location will not have any significant effects in terms of landscape or townscape in the context of the wider development under construction.

	3.3 Create places, spaces and buildings that work well, wear well and look good	0 Effects will depend on the final masterplan
Climate change and pollution	4.1 Reduce emissions of greenhouse gasses and other pollutants (including air, water, soil, noise, vibration and light)	++ Arbury Park is located within the built-up area of Cambridge. It is served by the Citi4 bus service, which runs at a frequency of 3 buses an hour. Currently Citi4 buses do not penetrate the development but the longer-term intention is that they will do.  A large proportion of Arbury Park is also within 800m of the route of the Citi1 bus service, which runs at a frequency of 6 buses an hour from Arbury.  Once the CGB is opened there will be additional bus services directly serving the Arbury Park development at a likely frequency of 6 buses an hour during the peak periods.  Arbury Park is reasonably connected to the wider Cambridge cycle network and is within walking distance of a number of the major employment areas at the Cambridge Science Park and Cambridge Business Park.
	4.2 Minimise waste production and support the recycling of waste products	0 At this stage there is no evidence to suggest how suitable development at this location will be for adopting sustainable waste management practices.
	4.3 Limit or reduce vulnerability to the effects of climate change (including flooding)	+ Development at this site will not lead to increased flood risk.

Healthy communities	5.1 Maintain and enhance human health	<p>-</p> <p>The site lies within the Air Quality Management Area (AQMA) along the A14. The proposal is for residential dwellings, which introduces receptors to the AQMA and has the potential to cause a negative impact on the AQMA. Monitoring of pollutants within the AQMA continues, although at present, results are not showing signs of consistent improvement.</p> <p>It would be vital that adequate and detailed low emissions policies were brought forward with any allocation so as to cause zero impact on the AQMA and ensure that a satisfactory residential environment can be created.</p> <p>Air quality will be a significant issue with regards to this site. It is recommended that this issue is raised and resolved prior to any allocation of the land.</p> <p>The site is immediately adjacent to the A14 lying to the north of the site which is proposed to be upgraded with improvements for an extra lane – moving it closer to site - and is also close to the Cambridge Guided Busway scheme.</p> <p>Traffic noise impact is therefore a serious consideration for any future residential uses in this area, although it is likely that effects can be mitigated to a significant degree through requirements for such things as noise barriers and double glazing.</p>
	5.2 Reduce and prevent crime, and reduce the fear of crime	<p>0</p> <p>There is no evidence to suggest whether this site is particularly suited or unsuited to address issues of crime or fear of crime.</p>
	5.3 Improve the quantity and quality of publicly accessible open space	<p>+</p> <p>An area of public open space is planned immediately adjoining the site.</p>
Inclusive communities	6.1 Improve the quality, range and accessibility of services and facilities (e.g. health, transport, education, training, leisure opportunities)	<p>+</p> <p>This site forms part of the wider Arbury development that includes provision of new services and facilities including a local centre.</p>
	6.2 Redress inequalities related to age, gender, disability, race, faith, location and income	<p>0</p> <p>This site is located close to the city centre, but it is not clear whether there is any need for social regeneration within the vicinity (that development at this site might contribute to).</p>
	6.3 Ensure all groups have access to decent, appropriate and affordable housing	<p>+</p> <p>This development sets out to meet housing need in an area (Cambridge City) where need is greatest (although South Cambridgeshire – the area surrounding Cambridge – also has significant housing needs).</p>



	6.4 Encourage and enable the active involvement of local people in community activities	+ There will be no implications for the character of the local area as the site forms a small part of the wider Arbury Park development site, which has already been allocated for development and is under construction.
Economic activity	7.1 Help people gain access to satisfying work appropriate to their skills, potential and place of residence	+ Site would form part of an urban extension to Cambridge and would have good access to a wide variety of employment opportunities, consistent with the strategy to provide more homes focused on Cambridge to improve the jobs:workers balance. It is located within walking distance of the major employment areas at the Cambridge Science Park and Cambridge Business Park.
	7.2 Support appropriate investment in people, places, communications and other infrastructure	0 There will be no significant effects in terms of this SA Objective
	7.3 Improve the efficiency, competitiveness, vitality and adaptability of the local economy	0 There will be no significant effects in terms of this SA Objective

**Summary**

There is little evidence that this proposed development site differs substantially from the other developments proposed at Arbury Park in terms of strategic sustainability considerations.

It forms part of a wider allocation carried forward from the Local Plan 2004 and included in the Submission Draft Site Specific Policies DPD and therefore subject to Sustainability Appraisal in the Final Sustainability Report 2006. The proposals are for different uses on certain parcels resulting in increase over 900 dwellings approved.

This wider Arbury Park site is already allocated for development and under construction. As such studies have already established that most environmental constraints are not a major consideration. The particular issues to be considered in the context of a change to residential use relate to nearby sources of noise and air pollution. The site is nearby to the A14, which is due to be upgraded, including adding an additional lane, and is within a designated Air Quality Management Area (AQMA). It will be particularly important to ensure that residential development at this location would not be introducing sensitive receptors (residents) to an area where their health would be at risk.

It will form part of the urban area of Cambridge and is in a sustainable location in terms of ensuring good accessibility and reducing car dependency. The site would have good access to local services and facilities in the wider Arbury Park development and employment in the nearby Science Park, as well as by good public transport provision served by a number of routes, including the guided busway.

**Recommendations**

If this location is to go forward for further consideration it will be important to:

1. Ensure that that there will be no risk to health and well-being as a result of exposure to noise and air pollution.

### Assessment table for Parcel Com 4, Arbury Park

SA Topic	SA Objective	Assessment findings
Land and water resources	1.1 Minimise the irreversible loss of undeveloped land and productive agricultural holdings	+? The site was formerly agricultural land (60% grade 2 & 40% grade 3), but has been cleared for development.
	1.2 Reduce the use of non-renewable resources including energy sources	0 There is no evidence to suggest whether this site is particularly suitable or unsuitable for sustainable design and construction and renewable energy technologies. Effects are expected to be dependent on the development of the masterplan.
	1.3 Limit water consumption to levels supportable by natural processes and storage systems	0 The absolute effect of further development will be to put additional strain on already stretched water resources in the sub-region, therefore possibly leading to lowered groundwater levels and impacts on natural processes. However, no particular constraints to this site in terms of water resources have been identified.
Biodiversity	2.1 Avoid damage to designated sites and protected species	+ Development at this location should not lead to significant impacts on protected species.
	2.2 Maintain and enhance the range and viability of characteristic habitats and species	+ Development at this location would not have any significant impacts on the range and viability of characteristic habitats and species.
	2.3 Improve opportunities for people to access and appreciate wildlife and wild places	? The nearest public right of way is Mere Way, starting around 200m to the east of the site, which passes through open countryside to Landbeach.
Landscape, townscape and archaeology	3.1 Avoid damage to areas and sites designated for their historic interest, and protect their settings	+ This location will avoid damage to areas and sites designated for their historic interest, and protect their settings
	3.2 Maintain and enhance the diversity and distinctiveness of landscape and townscape character	-? Impact on urban edge of Cambridge will be a relevant consideration, particularly as development at this location would result in need to retain a greater length of the acoustic barrier.
	3.3 Create places, spaces and buildings that work well, wear well and look good	0 Effects will depend on the final masterplan

Climate change and pollution	4.1 Reduce emissions of greenhouse gasses and other pollutants (including air, water, soil, noise, vibration and light)	<p>++</p> <p>Arbury Park is located within the built-up area of Cambridge. It is served by the Citi4 bus service, which runs at a frequency of 3 buses an hour. Currently Citi4 buses do not penetrate the development but the longer-term intention is that they will do.</p> <p>A large proportion of Arbury Park is also within 800m of the route of the Citi1 bus service, which runs at a frequency of 6 buses an hour from Arbury.</p> <p>Once the CGB is opened there will be additional bus services directly serving the Arbury Park development at a likely frequency of 6 buses an hour during the peak periods.</p> <p>Arbury Park is reasonably connected to the wider Cambridge cycle network and is within walking distance of a number of the major employment areas at the Cambridge Science Park and Cambridge Business Park.</p>
	4.2 Minimise waste production and support the recycling of waste products	<p>0</p> <p>At this stage there is no evidence to suggest how suitable development at this location will be for adopting sustainable waste management practices.</p>
	4.3 Limit or reduce vulnerability to the effects of climate change (including flooding)	<p>+</p> <p>Development at this site will not lead to increased flood risk.</p>

Healthy communities	5.1 Maintain and enhance human health	<p>-</p> <p>The site lies within the Air Quality Management Area (AQMA) along the A14. The proposal is for residential dwellings, which introduces receptors to the AQMA and has the potential to cause a negative impact on the AQMA. Monitoring of pollutants within the AQMA continues, although at present, results are not showing signs of consistent improvement.</p> <p>It would be vital that adequate and detailed low emissions policies were bought forward with any allocation so as to cause zero impact on the AQMA and ensure that a satisfactory residential environment can be created.</p> <p>Air quality will be a significant issue with regards to this site. It is recommended that this issue is raised and resolved prior to any allocation of the land.</p> <p>The site is immediately adjacent to the A14 lying to the north of the site which is proposed to be upgraded with improvements for an extra lane – moving it closer to site - and is also close to the Cambridge Guided Busway scheme.</p> <p>Traffic noise impact is therefore a serious consideration for any future residential uses in this area, although it is likely that effects can be mitigated to a significant degree through requirements for such things as noise barriers and double glazing.</p>
	5.2 Reduce and prevent crime, and reduce the fear of crime	<p>0</p> <p>There is no evidence to suggest whether this site is particularly suited or unsuited to address issues of crime or fear of crime.</p>
	5.3 Improve the quantity and quality of publicly accessible open space	<p>+</p> <p>An area of public open space is planned immediately adjoining the site.</p>
Inclusive communities	6.1 Improve the quality, range and accessibility of services and facilities (e.g. health, transport, education, training, leisure opportunities)	<p>+</p> <p>This site forms part of the wider Arbury development that includes provision of new services and facilities including a local centre.</p>
	6.2 Redress inequalities related to age, gender, disability, race, faith, location and income	<p>0</p> <p>This site is located close to the city centre, but it is not clear whether there is any need for social regeneration within the vicinity (that development at this site might contribute to).</p>
	6.3 Ensure all groups have access to decent, appropriate and affordable housing	<p>+</p> <p>This development sets out to meet housing need in an area (Cambridge City) where need is greatest (although South Cambridgeshire – the area surrounding Cambridge – also has significant housing needs).</p>

	6.4 Encourage and enable the active involvement of local people in community activities	+ There will be no implications for the character of the local area as the site forms a small part of the wider Arbury Park development site, which has already been allocated for development and is under construction.
Economic activity	7.1 Help people gain access to satisfying work appropriate to their skills, potential and place of residence	+ Site would form part of an urban extension to Cambridge and would have good access to a wide variety of employment opportunities, consistent with the strategy to provide more homes focused on Cambridge to improve the jobs:workers balance. It is located within walking distance of the major employment areas at the Cambridge Science Park and Cambridge Business Park.
	7.2 Support appropriate investment in people, places, communications and other infrastructure	0 There will be no significant effects in terms of this SA Objective
	7.3 Improve the efficiency, competitiveness, vitality and adaptability of the local economy	0 There will be no significant effects in terms of this SA Objective

**Summary**

There is very little evidence that this proposed development site differs substantially from the other developments proposed at Arbury Park in terms of strategic sustainability considerations, although noise pollution is perhaps a marginally greater (even more significant) constraint at this site.

It forms part of a wider allocation carried forward from the Local Plan 2004 and included in the Submission Draft Site Specific Policies DPD and therefore subject to Sustainability Appraisal in the Final Sustainability Report 2006. The proposals are for different uses on certain parcels resulting in increase over 900 dwellings approved.

This wider Arbury Park site is already allocated for development and under construction. As such studies have already established that most environmental constraints are not a major consideration. The only exceptions relate to nearby sources of noise and air pollution. The site is nearby to the A14, which is due to be expanded, and is within a designated Air Quality Management Area (AQMA). It will be particularly important to ensure that residential development at this location would not be introducing sensitive receptors (residents) to an area where their health would be at risk.

It will form part of the urban area of Cambridge and is in a sustainable location in terms of ensuring good accessibility and reducing car dependency. The site would have good access to local services and facilities in the wider Arbury Park development and employment in the nearby Science Park, as well as by good public transport provision served by a number of routes, including the guided busway.

**Recommendations**

If this location is to go forward for further consideration it will be important to:

1. Ensure that that there will be no risk to health and well-being as a result of exposure to noise and air pollution.

### Assessment table for Parcel Q and H.R.C.C, Arbury Park

SA Topic	SA Objective	Assessment findings
Land and water resources	1.1 Minimise the irreversible loss of undeveloped land and productive agricultural holdings	+? The site was formerly agricultural land (grade 3), but has been cleared for development.
	1.2 Reduce the use of non-renewable resources including energy sources	0 There is no evidence to suggest whether this site is particularly suitable or unsuitable for sustainable design and construction and renewable energy technologies. Effects are expected to be dependent on the development of the masterplan.
	1.3 Limit water consumption to levels supportable by natural processes and storage systems	0 The absolute effect of further development will be to put additional strain on already stretched water resources in the sub-region, therefore possibly leading to lowered groundwater levels and impacts on natural processes. However, no particular constraints to this site in terms of water resources have been identified.
Biodiversity	2.1 Avoid damage to designated sites and protected species	+ Development at this location should not lead to significant impacts on protected species.
	2.2 Maintain and enhance the range and viability of characteristic habitats and species	+ Development at this location would not have any significant impacts on the range and viability of characteristic habitats and species.
	2.3 Improve opportunities for people to access and appreciate wildlife and wild places	? The nearest public right of way is Mere Way, starting around 200m to the east of the site, which passes through open countryside to Landbeach.
Landscape, townscape and archaeology	3.1 Avoid damage to areas and sites designated for their historic interest, and protect their settings	? This site is located within the boundary of Arbury Camp ringwork, the location of which is reflected in the development masterplan. Any development in this area should therefore take account of and reflect its location within the ringwork.
	3.2 Maintain and enhance the diversity and distinctiveness of landscape and townscape character	+? Development at this location will not have any significant effects in terms of landscape or townscape although there is a need to consider the potential for development to retain open frontage to link open space to north and PVAA to south.
	3.3 Create places, spaces and buildings that work well, wear well and look good	0 Effects will depend on the final masterplan

Climate change and pollution	4.1 Reduce emissions of greenhouse gasses and other pollutants (including air, water, soil, noise, vibration and light)	<p>++</p> <p>Arbury Park is located within the built-up area of Cambridge. It is served by the Citi4 bus service, which runs at a frequency of 3 buses an hour. Currently Citi4 buses do not penetrate the development but the longer-term intention is that they will do.</p> <p>A large proportion of Arbury Park is also within 800m of the route of the Citi1 bus service, which runs at a frequency of 6 buses an hour from Arbury.</p> <p>Once the CGB is opened there will be additional bus services directly serving the Arbury Park development at a likely frequency of 6 buses an hour during the peak periods.</p> <p>Arbury Park is reasonably connected to the wider Cambridge cycle network and is within walking distance of a number of the major employment areas at the Cambridge Science Park and Cambridge Business Park.</p>
	4.2 Minimise waste production and support the recycling of waste products	<p>0</p> <p>At this stage there is no evidence to suggest how suitable development at this location will be for adopting sustainable waste management practices.</p>
	4.3 Limit or reduce vulnerability to the effects of climate change (including flooding)	<p>+</p> <p>Development at this site will not lead to increased flood risk.</p>

Healthy communities	5.1 Maintain and enhance human health	<p>-</p> <p>The site lies within the Air Quality Management Area (AQMA) along the A14. The proposal is for residential dwellings, which introduces receptors to the AQMA and has the potential to cause a negative impact on the AQMA. Monitoring of pollutants within the AQMA continues, although at present, results are not showing signs of consistent improvement.</p> <p>It would be vital that adequate and detailed low emissions policies were brought forward with any allocation so as to cause zero impact on the AQMA and ensure that a satisfactory residential environment can be created.</p> <p>Air quality will be a significant issue with regards to this site. It is recommended that this issue is raised and resolved prior to any allocation of the land.</p> <p>The site is immediately adjacent to the A14 lying to the north of the site which is proposed to be upgraded with improvements for an extra lane – moving it closer to site - and is also close to the Cambridge Guided Busway scheme.</p> <p>Traffic noise impact is therefore a serious consideration for any future residential uses in this area, although it is likely that effects can be mitigated to a significant degree through requirements for such things as noise barriers and double glazing.</p>
	5.2 Reduce and prevent crime, and reduce the fear of crime	<p>0</p> <p>There is no evidence to suggest whether this site is particularly suited or unsuited to address issues of crime or fear of crime.</p>
	5.3 Improve the quantity and quality of publicly accessible open space	<p>+</p> <p>An area of public open space is planned immediately adjoining the site.</p>
Inclusive communities	6.1 Improve the quality, range and accessibility of services and facilities (e.g. health, transport, education, training, leisure opportunities)	<p>+</p> <p>This site forms part of the wider Arbury development that includes provision of new services and facilities including a local centre.</p>
	6.2 Redress inequalities related to age, gender, disability, race, faith, location and income	<p>0</p> <p>This site is located close to the city centre, but it is not clear whether there is any need for social regeneration within the vicinity (that development at this site might contribute to).</p>
	6.3 Ensure all groups have access to decent, appropriate and affordable housing	<p>+</p> <p>This development sets out to meet housing need in an area (Cambridge City) where need is greatest (although South Cambridgeshire – the area surrounding Cambridge – also has significant housing needs).</p>



	6.4 Encourage and enable the active involvement of local people in community activities	+ There will be no implications for the character of the local area as the site forms a small part of the wider Arbury Park development site, which has already been allocated for development and is under construction.
Economic activity	7.1 Help people gain access to satisfying work appropriate to their skills, potential and place of residence	+ Site would form part of an urban extension to Cambridge and would have good access to a wide variety of employment opportunities, consistent with the strategy to provide more homes focused on Cambridge to improve the jobs:workers balance. It is located within a 2km walk of the major employment areas at the Cambridge Science Park and Cambridge Business Park.
	7.2 Support appropriate investment in people, places, communications and other infrastructure	0 There will be no significant effects in terms of this SA Objective
	7.3 Improve the efficiency, competitiveness, vitality and adaptability of the local economy	0 There will be no significant effects in terms of this SA Objective

**Summary**

There is very little evidence that this proposed development site differs substantially from the other developments proposed at Arbury Park in terms of strategic sustainability considerations. Compared to the other two proposals, this site is slightly further from the major employment development at the Science Park and is perhaps slightly less constrained in terms of noise and air pollution, although the closer proximity to A14 junction and heavy traffic levels on Cambridge Road will be significant.

This wider Arbury Park site is already allocated for development and under construction. As such studies have already established that most environmental constraints are not a major consideration. The only exceptions relate to nearby sources of noise and air pollution. The site is nearby to the A14, which is due to be expanded, and is within a designated Air Quality Management Area (AQMA). It will be particularly important to ensure that residential development at this location would not be introducing sensitive receptors (residents) to an area where their health would be at risk.

It will form part of the urban area of Cambridge and is in a sustainable location in terms of ensuring good accessibility and reducing car dependency. The site would have good access to local services and facilities in the wider Arbury Park development and employment in the nearby Science Park, as well as by good public transport provision served by a number of routes, including the guided busway.

**Recommendations**

If this location is to go forward for further consideration it will be important to:

1. Ensure that there will be no risk to health and well-being as a result of exposure to noise and air pollution.

### Assessment table for North West Cambridge AAP

SA Topic	SA Objective	Assessment findings
Land and water resources	1.1 Minimise the irreversible loss of undeveloped land and productive agricultural holdings	- The land is mainly agricultural land (Grade 2 and 3). There is a small amount of previously developed land.
	1.2 Reduce the use of non-renewable resources including energy sources	+? There is a local evidence base prepared as a supporting document to the AAP pursuant to PPS1 Climate Change Supplement which demonstrates that there are specific opportunities at North West Cambridge to set standards in the AAP that are in advance of building regulations changes, in terms of sustainability. However, it is not clear that development at other sites, particularly if the development was to be of a similar scale, would not have a similar potential to deliver similarly high standards.
	1.3 Limit water consumption to levels supportable by natural processes and storage systems	0 The absolute effect of further development will be to put additional strain on already stretched water resources in the sub-region, therefore possibly leading to lowered groundwater levels and impacts on natural processes. However, no particular constraints to this site in terms of water resources have been identified.
Biodiversity	2.1 Avoid damage to designated sites and protected species	? Ecological surveys have been carried out on the site, which have identified the presence of a range of protected species. It is likely that these issues can be addressed through mitigation.
	2.2 Maintain and enhance the range and viability of characteristic habitats and species	? Although the majority of the NWCAAP site comprises large areas of habitat of limited intrinsic nature conservation value, parts of the site have been found to be of some importance for protected species and other species of conservation concern.  Ecological studies have concluded that there may be opportunities for habitat enhancement and creation, mitigating for the potential impacts on farmland species in the northern and central parts of the site. However, the term enhancement must be used with caution. The key aim must be to ensure that existing species and habitats of conservation concern are not lost or impacted upon unduly, or replaced with habitats and species assemblages of lesser conservation value. The aim should not be simply to 'increase biodiversity' at the site level. Rather, the value of this land to species populations in the wider landscape must be maintained and enhanced.

	2.3 Improve opportunities for people to access and appreciate wildlife and wild places	<p>+</p> <p>There are Rights of Way crossing the north west part of the site and M11 leading to the countryside. The development of the masterplan is expected to provide improved linkages to the countryside. The Barton Road cycleway provides a safe cycle route as far as the village of Toft. A footpath on the western boundary of the site extends across the M11 motorway and links to the Cotton Countryside Reserve, effectively a Country park being developed by the Cambridge Preservation Society.</p>
Landscape, townscape and archaeology	3.1 Avoid damage to areas and sites designated for their historic interest, and protect their settings	<p>?-</p> <p>No listed buildings have been identified within the site. The site does lie opposite Girton College, but much of the site is behind existing properties on Huntingdon Road. Any development proposal would need to ensure that there will not be any unacceptable adverse impact on its setting, particularly in the NW part of the site which is immediately opposite the College.</p> <p>The site is likely to have significant archaeological value, and so this will require further investigation.</p>
	3.2 Maintain and enhance the diversity and distinctiveness of landscape and townscape character	<p>--</p> <p>This area is currently within the Green Belt. However, as identified in the Submission Draft North West Cambridge Area Action Plan, there is a need to balance the protection of this important setting of the City with meeting the development needs of Cambridge University. This major urban extension to Cambridge will be highly visible in the landscape from the west in short and medium distant views. It is likely that the new Green Belt boundary proposed will ensure that an adequate, open countryside setting to Cambridge is retained.</p> <p>The site currently provides an open setting to the village of Girton. Whilst it is a large development relative to Girton, the plans for the site seek to ensure that it will not adversely affect this necklace village.</p>
	3.3 Create places, spaces and buildings that work well, wear well and look good	<p>0</p> <p>Effects will depend on the final masterplan</p>
Climate change and pollution	4.1 Reduce emissions of greenhouse gasses and other pollutants (including air, water, soil, noise, vibration and light)	<p>+</p> <p>This is in an area where travelling distances are often likely to be short, and so there will be considerable potential to facilitate a modal shift away from car dependency. Furthermore, the site will consist of 50% University key worker housing giving it a Cambridge travel focus.</p>
	4.2 Minimise waste production and support the recycling of waste products	<p>0</p> <p>At this stage there is no evidence to suggest how suitable development at this location will be for adopting sustainable waste management practices.</p>

	4.3 Limit or reduce vulnerability to the effects of climate change (including flooding)	<p>?</p> <p>In 2005 the Strategic Flood Risk Assessment identified a significant risk of flooding to properties downstream in Girton and Oakington. Furthermore, the 2008 Water Cycle Strategy highlights the potential cumulative impact of development in the local catchment.</p> <p>Although the site is not in a flood risk zone the impact on the wider area should be considered in the development of the master plan. Because recent (2008) evidence has highlighted the vulnerability of the local catchment this effect has been scored as uncertain, whilst the SA of the North West Cambridge Area Action Plan (undertaken prior to this evidence becoming available) deemed flood risk not to be a significant sustainability issue.</p>
Healthy communities	5.1 Maintain and enhance human health	<p>?</p> <p>The location might suggest that development has the potential to generate a significant amount of extra traffic on the A14 and M11 and therefore worsen the existing AQMA on the A14. However, this effect will be mitigated substantially by the fact that the development will have a strong Cambridge focus.</p> <p>Existing uses of University Farm and Research sites would require investigation for contamination and potentially remediation.</p> <p>This site is in a location that may well encourage walking and cycling.</p>
	5.2 Reduce and prevent crime, and reduce the fear of crime	<p>0</p> <p>There is no evidence to suggest whether this site is particularly suited or unsuited to address issues of crime or fear of crime.</p>
	5.3 Improve the quantity and quality of publicly accessible open space	<p>+</p> <p>The site taken with land in Cambridge City intentionally has a large central open space to provide local open space as well as the linkages to wider countryside. Whilst much of that area is in Cambridge City it is all part of the AAP and is relevant to this assessment.</p>
Inclusive communities	6.1 Improve the quality, range and accessibility of services and facilities (e.g. health, transport, education, training, leisure opportunities)	<p>++</p> <p>This is a large site and a mixed use development is proposed with new local services and community facilities to serve the development</p> <p>New public transport routes can be delivered as part of the development, with the result being high quality public transport with the majority of homes being within 400m easy walking distance of a public transport stop.</p> <p>The scale of development is such that it can provide its own new local centre and primary school. A new secondary school to serve the wider north west part of Cambridge is proposed by the County Council to be located nearby.</p>

	6.2 Redress inequalities related to age, gender, disability, race, faith, location and income	0 This site is located within the city, but adjoining parts of Cambridge are generally quite affluent.
	6.3 Ensure all groups have access to decent, appropriate and affordable housing	++ This development sets out to meet a specific housing need
	6.4 Encourage and enable the active involvement of local people in community activities	+ The development of this site will allow a community to develop of core university workers, who might otherwise not be able to afford to live in Cambridge.
Economic activity	7.1 Help people gain access to satisfying work appropriate to their skills, potential and place of residence	+ Site would form an urban extension to Cambridge and would have good access to a wide variety of employment opportunities, consistent with the strategy to provide more homes focused on Cambridge to improve the jobs:workers balance. The site is located 3km from Cambridge City Centre, 6km from the Science Park and 7km Addenbrokes, which are key locations for employment. The site will also include new employment with links to the University.
	7.2 Support appropriate investment in people, places, communications and other infrastructure	0 There will be no significant effects in terms of this SA Objective
	7.3 Improve the efficiency, competitiveness, vitality and adaptability of the local economy	0 There will be no significant effects in terms of this SA Objective
<p><b>Summary</b></p> <p>This is a greenfield site and there are a range of important considerations in terms of the ecological and landscape value of the land at this location. In terms of landscape, the setting of Cambridge will certainly suffer somewhat as a result of development here. Furthermore, there is a need to ensure no increase in known flood risk downstream.</p> <p>The site has potential to create a sustainable urban extension to Cambridge in terms ensuring good access to services and facilities and promoting sustainable modes of transport. This is a large site and a mixed use development is proposed with new local services and community facilities to serve the development.</p> <p>The site would also have good access to a wide variety of employment opportunities, consistent with the strategy to provide more homes focused on Cambridge to improve the jobs workers balance. There are also considerable sustainability benefits to consider relating to the fact that the development sets out to meet a specific housing need (that of university workers).</p> <p><b>Recommendations</b></p> <p>If this location is to go forward for further consideration it will be important to:</p>		

1. Ensure that the biodiversity value of the site and the surrounding landscape is not significantly reduced.
2. Ensure that landscape effects are mitigated as far as possible.
3. Ensure that flood risk downstream does not increase.

**Assessment table for land between Huntingdon Road, Histon Road and the A14, North West Cambridge (Objector's site)**

SA Topic	SA Objective	Assessment findings
Land and water resources	1.1 Minimise the irreversible loss of undeveloped land and productive agricultural holdings	- The land is mainly agricultural land (Grade 2 and 3). There is a small amount of previously developed land.
	1.2 Reduce the use of non-renewable resources including energy sources	0 There is no evidence to suggest whether this site is particularly suitable or unsuitable for sustainable design and construction and renewable energy technologies. Effects are expected to be dependent on the development of the masterplan.
	1.3 Limit water consumption to levels supportable by natural processes and storage systems	0 The absolute effect of further development will be to put additional strain on already stretched water resources in the sub-region, therefore possibly leading to lowered groundwater levels and impacts on natural processes. However, no particular constraints to this site in terms of water resources have been identified.
Biodiversity	2.1 Avoid damage to designated sites and protected species	? The site supports a number of protected species including badgers, bats, water voles and brown hare.
	2.2 Maintain and enhance the range and viability of characteristic habitats and species	? The majority of the site comprises arable land considered to be of local ecological value. There is a strong hedge line running through the site along the southern edge of the area proposed for development by the objector. A drainage ditch runs broadly parallel with the NW boundary of the area proposed for development, within the development area.  Retention of habitat important for wildlife would be necessary, e.g. hedgerows used by bats and ditches used by water voles should be considered by the development of the final masterplan.
	2.3 Improve opportunities for people to access and appreciate wildlife and wild places	+? A private agricultural road runs through the objection site and links with land to the countryside north of the A14 via a bridge which has potential to provide public access.
Landscape, townscape and archaeology	3.1 Avoid damage to areas and sites designated for their historic interest, and protect their settings	? The site may have significant archaeological value, and so this will require further investigation.

	3.2 Maintain and enhance the diversity and distinctiveness of landscape and townscape character	-- There are views across this open agricultural area from the A14 towards Cambridge and it provides an open countryside setting to the City and Girton village. The scale of development might not have a significant harmful impact on the overall character of Cambridge, but would have an impact on the character of Girton by reducing the separation with the city.
	3.3 Create places, spaces and buildings that work well, wear well and look good	0 Effects will depend on the final masterplan
Climate change and pollution	4.1 Reduce emissions of greenhouse gasses and other pollutants (including air, water, soil, noise, vibration and light)	+ The NW Cambridge Transport Study anticipated that a relatively high share of sustainable modes could be achieved by developments in this area, with potentially less than 40% of trips by private car.
	4.2 Minimise waste production and support the recycling of waste products	0 At this stage there is no evidence to suggest how suitable development at this location will be for adopting sustainable waste management practices.
	4.3 Limit or reduce vulnerability to the effects of climate change (including flooding)	?- Strategic Flood Risk Assessment has identified a risk of flooding associated with the drainage ditch which runs through the proposed development area. Even though this is a small ditch that effects a very small part of the site it may be that mitigation measures are required.
Healthy communities	5.1 Maintain and enhance human health	- The north of the site abuts the Air Quality Management Area (AQMA) designated along the A14 and the site. It will be important that any risk to human health as a result of development at this location is avoided and that the development does not contribute to the poor air quality or extend the area of the air quality problem. The proposal has the potential to generate a significant amount of extra traffic on the A14 and surrounding distributor roads.  The site is in close proximity to the A14 to the North of site is bounded by the M11 and A14, which will be sources of noise pollution. It is likely that this effect can be adequately mitigated, although mitigation measures may be constrained to a degree by landscape constraints.  There is potential for public access to the countryside via existing farm routes, and the expectation is that since the site is located close to the city centre there might be good access by bicycle, therefore promoting cycling.
	5.2 Reduce and prevent crime, and reduce the fear of crime	0 There is no evidence to suggest whether this site is particularly suited or unsuited to addressing issues of crime or fear of crime.



	5.3 Improve the quantity and quality of publicly accessible open space	?+ There is potential for relatively good access to the countryside if private farm routes are made available to the public. There is no indication regarding the provision of open space but it is expected that it would need to meet local standards.
Inclusive communities	6.1 Improve the quality, range and accessibility of services and facilities (e.g. health, transport, education, training, leisure opportunities)	+? The site has potential to form part of a larger sustainable urban extension to Cambridge together with land already allocated in Cambridge City. The adjacent development site would bring forward significant improvements to both services and facilities and public transport close to the site and could be further enhanced either by additional services and facilities located within this site or by financial contributions brought forward as a result of development at this site that could enhance the proposed local centre in the adjoining site. The Highways Agency have indicated that prior to the completion of the A14 improvements there will be significant transport constraints
	6.2 Redress inequalities related to age, gender, disability, race, faith, location and income	0 This site is located within the city, but as there are no particular known problems of deprivation in adjoining parts of Cambridge it is not clear that development at this location would contribute to any aims of regeneration..
	6.3 Ensure all groups have access to decent, appropriate and affordable housing	+ This development sets out to meet housing need in an area (Cambridge City) where need is greatest (although South Cambridgeshire – the area surrounding Cambridge – also has significant housing needs).
	6.4 Encourage and enable the active involvement of local people in community activities	0 It is not thought that development at this site will have any particular community impacts.
Economic activity	7.1 Help people gain access to satisfying work appropriate to their skills, potential and place of residence	+ The site has the benefit of providing housing on the edge of Cambridge to help improve the balance between jobs and workers in Cambridge
	7.2 Support appropriate investment in people, places, communications and other infrastructure	0 There will be no significant effects in terms of this SA Objective
	7.3 Improve the efficiency, competitiveness, vitality and adaptability of the local economy	0 There will be no significant effects in terms of this SA Objective
<p><b>Summary</b> This is a greenfield site and there are a range of important considerations in terms of the ecological and landscape value of the land at this location. The loss of Green Belt is an important consideration but not a fundamental constraint to development at this site. There are no other absolute constraints, although</p>		

there is a degree of flood risk that must be mitigated and the potential to worsen existing poor air quality is an important consideration. There is also a need to ensure no increase in known flood risk downstream.

The site would also have good access to a wide variety of employment opportunities, consistent with the strategy to provide more homes focused on Cambridge to improve the jobs workers balance. The site has potential to create a sustainable urban extension to Cambridge in terms of ensuring good access to services and facilities and promoting sustainable modes of transport. The site has potential to form part of a larger sustainable urban extension to Cambridge together with land already allocated in Cambridge City. The adjacent development site would bring forward significant improvements to both services and facilities and public transport close to the site and could be further enhanced either by additional services and facilities located within this site or by financial contributions brought forward as a result of development at this site.

### **Recommendations**

If this location is to go forward for further consideration it will be important to:

1. Ensure that the biodiversity value of the site and the surrounding landscape is not significantly reduced.
2. Mitigate landscape impacts through the scale and footprint of any development and the landscaping and design of the development.
3. Ensure that flood risk does not increase.
4. Ensure that air poor air quality does not impact on health

**Assessment table for land between Huntingdon Road, Histon Road and the A14, North West Cambridge (South Cambs Revision)**

SA Topic	SA Objective	Assessment findings
Land and water resources	1.1 Minimise the irreversible loss of undeveloped land and productive agricultural holdings	- The land is mainly agricultural land (Grade 2 and 3). There is a small amount of previously developed land.
	1.2 Reduce the use of non-renewable resources including energy sources	0 There is no evidence to suggest whether this site is particularly suitable or unsuitable for sustainable design and construction and renewable energy technologies. Effects are expected to be dependent on the development of the masterplan.
	1.3 Limit water consumption to levels supportable by natural processes and storage systems	0 The absolute effect of further development will be to put additional strain on already stretched water resources in the sub-region, therefore possibly leading to lowered groundwater levels and impacts on natural processes. However, no particular constraints to this site in terms of water resources have been identified.
Biodiversity	2.1 Avoid damage to designated sites and protected species	? The site supports a number of protected species including badgers, bats, water voles and brown hare.
	2.2 Maintain and enhance the range and viability of characteristic habitats and species	? The majority of the site comprises arable land considered to be of local ecological value. There is a strong hedgeline running through the site along the southern edge of the area proposed for development by the objector. A drainage ditch runs broadly parallel with the NW boundary of the area proposed for development, within the development area.  Retention of habitat important for wildlife would be necessary, e.g. hedgerows used by bats and ditches used by water voles should be considered by the development of the final masterplan.
	2.3 Improve opportunities for people to access and appreciate wildlife and wild places	+? A private agricultural road runs through the objection site and links with land to the countryside north of the A14 via a bridge which has potential to provide public access.
Landscape, townscape and archaeology	3.1 Avoid damage to areas and sites designated for their historic interest, and protect their settings	? The site may have significant archaeological value, and so this will require further investigation.

	3.2 Maintain and enhance the diversity and distinctiveness of landscape and townscape character	- There are views across this open agricultural area from the A14 towards Cambridge and it provides an open countryside setting to the City and Girton village. The scale of development might not have a significant harmful impact on the overall character of Cambridge, but would have an impact on the character of Girton by reducing the separation with the city. However, this revised site footprint seeks to reduce the impact on Green Belt purposes, and impacts can be further mitigated through the landscaping and design of the development.
	3.3 Create places, spaces and buildings that work well, wear well and look good	0 Effects will depend on the final masterplan
Climate change and pollution	4.1 Reduce emissions of greenhouse gasses and other pollutants (including air, water, soil, noise, vibration and light)	+ The NW Cambridge Transport Study anticipated that a relatively high share of sustainable modes could be achieved by developments in this area, with potentially less than 40% of trips by private car.
	4.2 Minimise waste production and support the recycling of waste products	0 At this stage there is no evidence to suggest how suitable development at this location will be for adopting sustainable waste management practices.
	4.3 Limit or reduce vulnerability to the effects of climate change (including flooding)	?- Strategic Flood Risk Assessment has identified a risk of flooding associated with the drainage ditch which runs along the northern boundary of the site. Even though this is a small ditch that effects a very small part of the site it may be that mitigation measures are required.
Healthy communities	5.1 Maintain and enhance human health	? This revised site footprint maintains a 30m buffer between the development and the Air Quality Management Area (AQMA) designated along the A14. It will be important that the development does not contribute to the poor air quality in the AQMA. The proposal has the potential to generate a significant amount of extra traffic on the A14 and surrounding distributor roads.  The site is in close proximity to the A14 to the North of site is bounded by the M11 and A14, which will be sources of noise pollution. It is likely that this effect can be adequately mitigated, although mitigation measures may be constrained to a degree by landscape constraints.  There is potential for public access to the countryside via existing farm routes, and the expectation is that since the site is located close to the city centre there might be good access by bicycle enhancing cycling.
	5.2 Reduce and prevent crime, and reduce the fear of crime	0 There is no evidence to suggest whether this site is particularly suited or unsuited to address issues of crime or fear of crime.

	5.3 Improve the quantity and quality of publicly accessible open space	? There is potential for relatively good access to the countryside if private farm routes are made available to the public. There is no indication regarding the provision of open space but it is expected that it would need to meet local standards.
Inclusive communities	6.1 Improve the quality, range and accessibility of services and facilities (e.g. health, transport, education, training, leisure opportunities)	+? The site has potential to form part of a larger sustainable urban extension to Cambridge together with land already allocated in Cambridge City. The adjacent development site would bring forward significant improvements to both services and facilities and public transport close to the site and could be further enhanced either by additional services and facilities located within this site or by financial contributions brought forward as a result of development at this site that could enhance the proposed local centre in the adjoining site. The Highways Agency have indicated that prior to the completion of the A14 improvements there will be significant transport constraints
	6.2 Redress inequalities related to age, gender, disability, race, faith, location and income	0 This site is located within the city, but as there are no particular known problems of deprivation in adjoining parts of Cambridge it is not clear that development at this location would contribute to any aims of regeneration..
	6.3 Ensure all groups have access to decent, appropriate and affordable housing	+ This development sets out to meet housing need in an area (Cambridge City) where need is greatest (although South Cambridgeshire – the area surrounding Cambridge – also has significant housing needs).
	6.4 Encourage and enable the active involvement of local people in community activities	0 It is not thought that development at this site will have any particular community impacts.
Economic activity	7.1 Help people gain access to satisfying work appropriate to their skills, potential and place of residence	+ The site has the benefit of providing housing on the edge of Cambridge to help improve the balance between jobs and workers in Cambridge
	7.2 Support appropriate investment in people, places, communications and other infrastructure	0 There will be no significant effects in terms of this SA Objective
	7.3 Improve the efficiency, competitiveness, vitality and adaptability of the local economy	0 There will be no significant effects in terms of this SA Objective
<p><b>Summary</b> This is a greenfield site and there are a range of important considerations in terms of the ecological and landscape value of the land at this location. The loss of Green Belt is an important consideration but not a fundamental constraint to development at this site. There are no other absolute constraints, although</p>		

there is a degree of flood risk that must be mitigated and the potential to worsen existing poor air quality is an important consideration. This revised site footprint should significantly reduce the potential for poor air quality to impact on health. There is also a need to ensure no increase in known flood risk downstream.

The site would also have good access to a wide variety of employment opportunities, consistent with the strategy to provide more homes focused on Cambridge to improve the jobs workers balance. The site has potential to create a sustainable urban extension to Cambridge in terms of ensuring good access to services and facilities and promoting sustainable modes of transport. The site has potential to form part of a larger sustainable urban extension to Cambridge together with land already allocated in Cambridge City. The adjacent development site would bring forward significant improvements to both services and facilities and public transport close to the site and could be further enhanced either by additional services and facilities located within this site or by financial contributions brought forward as a result of development at this site.

### **Recommendations**

If this location is to go forward for further consideration it will be important to:

1. Ensure that the biodiversity value of the site and the surrounding landscape is not significantly reduced.
2. Mitigate landscape impacts through the scale and footprint of any development and the landscaping and design of the development.
3. Ensure that flood risk does not increase.

### Assessment table for land north of Barton Rd.

SA Topic	SA Objective	Assessment findings
Land and water resources	1.1 Minimise the irreversible loss of undeveloped land and productive agricultural holdings	- The land to be built upon comprises an area of small agricultural fields (Grade 3) with a small area of farm buildings
	1.2 Reduce the use of non-renewable resources including energy sources	0 There is no evidence to suggest whether this site is particularly suitable or unsuitable for sustainable design and construction and renewable energy technologies. Effects are expected to be dependent on the development of the masterplan.
	1.3 Limit water consumption to levels supportable by natural processes and storage systems	0 The absolute effect of further development will be to put additional strain on already stretched water resources in the sub-region, therefore possibly leading to lowered groundwater levels and impacts on natural processes. However, no particular constraints to this site in terms of water resources have been identified.
Biodiversity	2.1 Avoid damage to designated sites and protected species	-? Protected species are present on the site (see 2.2)
	2.2 Maintain and enhance the range and viability of characteristic habitats and species	?- The part of the site proposed for development comprises mainly arable land and semi-improved grassland. A phase 1 habitat survey carried out in 2004 noted that otters, water voles, badgers, foxes, deer, and a variety of birds use the site. It is also suitable for bats and reptiles. The Barton Road frontage contains a number of broad-leaved trees, and the remnants of an orchard. There are also a number of hedgerows, including one that broadens into a tree belt. There are a number of wet ditches present, including the Bin Brook which runs along the Barton Road frontage, noted to be of high value due to the presence of water voles.  The phase 1 study recommended retention of the semi-improved grassland and orchards, and to retain and enhance ditch habitat. If the site were allocated for development an updated survey would be required. It will be important to consider the value of the site as a whole, and not to automatically assume that the value of site, in terms of supporting populations of species in the wider landscape, can automatically be maintained just by the retention and incorporation into the development of certain features.

	2.3 Improve opportunities for people to access and appreciate wildlife and wild places	<p>+</p> <p>The site has good potential for countryside access using existing rights of way. The Barton Road cycleway provides a safe cycle route as far as the village of Toft. A footpath on the western boundary of the site extends across the M11 motorway and links to the Coton Countryside Reserve, effectively a Country park being development by the Cambridge Preservation Society.</p>
Landscape, townscape and archaeology	3.1 Avoid damage to areas and sites designated for their historic interest, and protect their settings	<p>+</p> <p>Development at this site would avoid damage to areas and sites designated for their historic interest, and protect their settings</p>
	3.2 Maintain and enhance the diversity and distinctiveness of landscape and townscape character	<p>-</p> <p>Whilst the site is small in size, development would nevertheless have an impact on this important part of the setting of the City. Previous studies have deemed the site to be of medium importance in terms of landscape. Existing trees and hedgerows provide some scope to mitigate the impact of development, particularly during the summer months. However, residential development would appear incongruous. Without land in Cambridge City and excluding land in flood zones 2 and 3, the developable area does not physically join to the built up area of Cambridge and would not read as an urban extension to Cambridge.</p>
	3.3 Create places, spaces and buildings that work well, wear well and look good	<p>0</p> <p>Effects will depend on the final masterplan</p>



Climate change and pollution	4.1 Reduce emissions of greenhouse gasses and other pollutants (including air, water, soil, noise, vibration and light)	<p>-?</p> <p>Adjacent to the site is an off-road cycleway which leads into the city centre and the city cycle network.</p> <p>If this site followed the pattern of the district/city wards within which it lies then a car driver mode share of between 27-56% could be expected. If the site could support its own bus service then a car driver mode share closer to 27% could be expected. However, if the site can not support its own bus service then a car driver mode share closer to 56%, or above, could be expected given that the site is both physically and physiologically separate from the built-up area of the City.</p> <p>In practice, it is likely that the scale of development likely to be feasible (i.e. outside flood zones 2 and 3) may well not support significant improvements to bus services.</p> <p>Bus services would also be likely to get caught in existing congestion, reducing reliability and attractiveness and extending journey times.</p> <p>Given the location of the site close to the M11, travel to sites such as Addenbrookes and the Science Park would be likely to be more attractive to be made by car. Furthermore, drivers wishing to use the Park and Ride would also need to drive via the M11 to Madingley Road or Trumpington.</p>
	4.2 Minimise waste production and support the recycling of waste products	<p>0</p> <p>At this stage there is no evidence to suggest how suitable development at this location will be for adopting sustainable waste management practices.</p>
	4.3 Limit or reduce vulnerability to the effects of climate change (including flooding)	<p>--</p> <p>Approximately 65% of the site lies within Zones 2 and 3. The Zone 3 land broadly follows the line of the Bin Brook watercourse alongside Barton Road. The remainder of the site sits in flood zone 1, and theoretically could be developed without the remainder of the site, but the proximity of the flood zones must still be an important consideration.</p>
Healthy communities	5.1 Maintain and enhance human health	<p>-</p> <p>Air quality could be a significant issue with regards to this site. The site is close to two AQMAs – (Cambridge city centre and the AQMA on the A14 to the north). This site adjoins the M11 and a major route into and out of Cambridge. Drivers wishing to access the Park &amp; Ride sites would also use the M11. It can be assumed that a significant amount of extra traffic may be generated during the operational phase of the development.</p> <p>Due to existing uses of the site contaminated land is a consideration that will require investigation and remedial as necessary.</p> <p>Accessing Cambridge by cycling is likely to be an attractive option.</p>

	5.2 Reduce and prevent crime, and reduce the fear of crime	0 There is no evidence to suggest whether this site is particularly suited or unsuited to reducing crime or fear of the crime.
	5.3 Improve the quantity and quality of publicly accessible open space	? There is relatively good access to the countryside. There is no indication regarding the provision of open space but it is anticipated it would need to meet local standards.
Inclusive communities	6.1 Improve the quality, range and accessibility of services and facilities (e.g. health, transport, education, training, leisure opportunities)	? Located so close to the built-up area of Cambridge, this level of development is unlikely to generate sufficient demand for any additional services and facilities (assuming a scale of development that avoids flood risk areas).  Adjacent to the site is an off-road cycleway which leads into the city centre and the city cycle network.  Given the level of planned housing here (maximum 400 dwellings) the most frequent bus service that could possibly be supported from the site is likely to be 2 buses an hour. However, if flood risk areas are excluded then a much lower level of development would be possible, which would affect potential for bus improvements.
	6.2 Redress inequalities related to age, gender, disability, race, faith, location and income	0 This has a weak physical connection to the built up area of Cambridge. It is not thought that development at this location would contribute to any aims of regeneration.
	6.3 Ensure all groups have access to decent, appropriate and affordable housing	+ This development sets out to meet housing need in an area (Cambridge City) where need is greatest (although South Cambridgeshire – the area surrounding Cambridge – also has significant housing needs).
	6.4 Encourage and enable the active involvement of local people in community activities	0 It is not thought that development at this site will have any particular community impacts.
Economic activity	7.1 Help people gain access to satisfying work appropriate to their skills, potential and place of residence	0? The site is close to Cambridge and would have good access to a wide variety of employment opportunities, consistent with the strategy to provide more homes focused on Cambridge to improve the jobs workers balance. However, poor quality public transport means that this site does not perform as well as other edge of Cambridge sites.
	7.2 Support appropriate investment in people, places, communications and other infrastructure	0 There will be no significant effects in terms of this SA Objective
	7.3 Improve the efficiency, competitiveness, vitality	0 There will be no significant effects in terms of this SA Objective

	and adaptability of the local economy	
<p><b>Summary</b></p> <p>This is a greenfield site with some ecological and landscape constraints to consider. Whilst the site is small in size, development would nevertheless have an impact on this important part of the Green Belt setting of the City. Air quality could be a significant issue as a result of the proximity of two AQMAs and the likelihood of increasing levels of traffic on already congested roads. Flood risk is also a major constraint at this site.</p> <p>The site has less potential to create a sustainable urban extension to Cambridge, in terms of ensuring good access to services and facilities and promoting sustainable modes of transport, than is the case for other sites on the edge of the City. This is primarily because of question marks over the potential to make public transport an attractive option as a mode of transport. Given the location of the site close to the M11, travel to sites such as the Park &amp; Ride sites, Addenbrookes and the Science Park would be likely to be more attractive to be made by car. However, adjacent to the site is an off-road cycleway which leads into the city centre and the city cycle network.</p> <p><b>Recommendations</b></p> <p>If this location is to go forward for further consideration it will be important to:</p> <ol style="list-style-type: none"><li>1. Ensure that adequate public transport can be accessed easily from the site</li><li>2. Ensure that biodiversity constraints are given adequate consideration</li><li>3. Ensure that flood risk on and off-site is minimised</li><li>4. Ensure that air poor air quality does not impact on health</li></ol>		

### Assessment table for Land north of Fen Road, Milton (Greengates Piggeries)

SA Topic	SA Objective	Assessment findings
Land and water resources	1.1 Minimise the irreversible loss of undeveloped land and productive agricultural holdings	? Site includes a number of agricultural style buildings and some areas of hard standing but mainly comprises open land of low agricultural value.
	1.2 Reduce the use of non-renewable resources including energy sources	0 There is no evidence to suggest whether this site is particularly suitable or unsuitable for sustainable design and construction and renewable energy technologies. Effects are expected to be dependent on the development of the masterplan.
	1.3 Limit water consumption to levels supportable by natural processes and storage systems	0? The absolute effect of further development will be to put additional strain on already stretched water resources in the sub-region, therefore possibly leading to lowered groundwater levels and impacts on natural processes. Also, in this area foul water drainage is known to be problematic and so there will be implications for ensuring the quality of water resources.
Biodiversity	2.1 Avoid damage to designated sites and protected species	+ Development at this location should not lead to significant impacts on protected species.
	2.2 Maintain and enhance the range and viability of characteristic habitats and species	? Much of the site retains a semi-natural character and would appear to have been under a grazing regime for many years. A phase 1 habitat survey would be required in order to provide further information on the range of habitats present within the site.  It is also considered that the hedgerows within and bounding the site provide opportunities for habitat linkage and further enhancements.  It will be important to consider whether the existing habitats on this site contribute to the viability of what is clearly an important ecological network running along the River Cam.
	2.3 Improve opportunities for people to access and appreciate wildlife and wild places	++ Access to diverse and biodiversity rich greenspace and countryside is good from this area. There are long distance footpaths along the River Cam, three local nature reserves within walking distance and Milton Country Park only about 1km away.
Landscape, townscape and archaeology	3.1 Avoid damage to areas and sites designated for their historic interest, and protect their settings	? The site is located in an area of high archaeological potential. The County Council would recommend that the site should be subject to a programme of archaeological investigation in advance of development, but would not necessarily object to development at this location.

	3.2 Maintain and enhance the diversity and distinctiveness of landscape and townscape character	-? Residential development of this site would change the character of this area, which retains a rural character, partly due to the separation of the railway line, both from Cambridge Northern Fringe East and from the Chesterton area of Cambridge. However, it is important to note that there is currently a significant amount of traveller related development and the site is adjacent to low key commercial development such as car repairs. Overall it is not an area of any particular landscape quality.
	3.3 Create places, spaces and buildings that work well, wear well and look good	0 Effects will depend on the final masterplan
Climate change and pollution	4.1 Reduce emissions of greenhouse gasses and other pollutants (including air, water, soil, noise, vibration and light)	? This development as a whole would be located approximately 600m from the nearest bus stops in East Chesterton, which has an hourly service to the city centre. The Citi2 service, which runs at a frequency of 6 buses an hour, is located 1.2 km from the site. A development of this size would not be able to support its own bus service or an extension to the existing service.  Cycling and walking should be fairly attractive options for accessing employment and the city centre, although there will be a need to cross the level-crossing. A relatively low car driver mode share (41% - 57%) could be expected.
	4.2 Minimise waste production and support the recycling of waste products	0 At this stage there is no evidence to suggest how suitable development at this location will be for adopting sustainable waste management practices.
	4.3 Limit or reduce vulnerability to the effects of climate change (including flooding)	- A substantial area of this site is mapped as Flood Zone 2 (Low to Medium Risk). Residential development could be focused on the area within flood zone 1, but the exact area would need to be determined by a site specific flood risk assessment. There are also concerns with regard to access to the site in times of flood, and it has not been demonstrated that appropriate access could be achieved. There are recorded historic details of flooding in this area.

Healthy communities	5.1 Maintain and enhance human health	<p>-</p> <p>The North West of the site is bounded by a mainline railway line and industrial type premises to the North East. Railway noise / vibration and noise from any commercial and industrial uses that will remain on the periphery will be an important consideration. It is uncertain whether mitigation measures on the site could provide an acceptable ambient noise environment.</p> <p>There are known areas of contamination within the area, which would need appropriate investigation and remediation where necessary.</p> <p>Milton sewage works are in relatively close proximity to the North and as a result an odour assessment is recommended.</p>
	5.2 Reduce and prevent crime, and reduce the fear of crime	<p>0</p> <p>There is no evidence to suggest whether this site is particularly suited or unsuited to address issues of crime or fear of crime.</p>
	5.3 Improve the quantity and quality of publicly accessible open space	<p>?</p> <p>This site appears to be some way from publicly accessible formal open space, although there is good access to the countryside.</p>
Inclusive communities	6.1 Improve the quality, range and accessibility of services and facilities (e.g. health, transport, education, training, leisure opportunities)	<p>-</p> <p>Small-scale development, unlikely to bring significant associated improvements in services and facilities. The development would not therefore in itself provide a sustainable mixed development and would rely on existing local services and facilities some distance from the site.</p> <p>The nearest supermarket and children's playspace are both over 1km away; the nearest and post office and outdoor sports pitch over 1.5km away; the nearest doctor's surgery about 2km away; and the nearest primary and secondary schools over 2.5km away.</p>
	6.2 Redress inequalities related to age, gender, disability, race, faith, location and income	<p>?-</p> <p>The site is currently designated in Local Plan 2004 saved policy CNF6, as an area where permission may be granted for private gypsy sites to meet local need. This policy will be reviewed by the Gypsy and Traveller DPD which is currently in preparation.</p> <p>It will be important to ensure that an opportunity to meet Gypsy and Traveller housing need is not lost without due consideration.</p>
	6.3 Ensure all groups have access to decent, appropriate and affordable housing	<p>+</p> <p>This development sets out to meet housing need in an area (Cambridge City) where need is greatest (although South Cambridgeshire – the area surrounding Cambridge – also has significant housing needs).</p>
	6.4 Encourage and enable the active involvement of local people in community activities	<p>0?</p> <p>It is not thought that development at this site will have any particular community impacts. However, it will be important to consider existing uses of the site.</p>

Economic activity	7.1 Help people gain access to satisfying work appropriate to their skills, potential and place of residence	+ Location of site on the edge of Cambridge would provide good access to a wide variety of employment opportunities, consistent with the strategy to provide more homes focused on Cambridge to improve the jobs:workers balance. The Science Park is only 3km away and Cambridge City Centre is 5km away.
	7.2 Support appropriate investment in people, places, communications and other infrastructure	0 There will be no significant effects in terms of this SA Objective
	7.3 Improve the efficiency, competitiveness, vitality and adaptability of the local economy	0 There will be no significant effects in terms of this SA Objective

**Summary**

This is a site with some areas of previously developed land, but which is mainly open land of low agricultural value. The site is in close proximity to the River Cam ecological corridor and may have some biodiversity value associated with this or as a result of other habitats and species present of local importance. The site has particularly good access to biodiversity rich greenspace, including a long-distance footpath along the River Cam. However, related to the proximity to the Cam, there is a degree of local flood-risk. Development could avoid the land at greatest risk, but a more detailed Flood Risk Assessment would be important. In terms of landscape, residential development of this site would change the character of this area, which retains something of a rural character. There are also constraints as a result of noise pollution from the nearby mainline railway.

Location of site on the edge of Cambridge would provide good access to a wide variety of employment opportunities, consistent with the strategy to provide more homes focused on Cambridge to improve the jobs:workers balance. There is a good local bus service into the city centre, but the nearest bus-stop for this service is located 1km away. Cycling and walking should be fairly attractive options for accessing employment and the city centre, although there will be a need to cross the level-crossing. However, local services and facilities are located a considerable distance away in Chesterton, and would not be easily accessible by walking.

The site is currently designated in Local Plan 2004 saved policy CNF6, as an area where permission may be granted for private gypsy sites to meet local need. This policy will be reviewed by the Gypsy and Traveller DPD which is currently in preparation.

**Recommendations**

If this location is to go forward for further consideration it will be important to:

1. Demonstrate that flood risk can be over-come through a detailed assessment.
2. Ensure that noise and odour pollution can be adequately avoided or mitigated.
3. Ensure that an opportunity to meet Gypsy and traveller housing need is not lost without due consideration

### Assessment table for Land south of Manor Park / Somerset Road (Histon)

SA Topic	SA Objective	Assessment findings
Land and water resources	1.1 Minimise the irreversible loss of undeveloped land and productive agricultural holdings	- Site comprises former agricultural land, now open grassland and woodland. 30% is classified as agricultural (Grade 2).
	1.2 Reduce the use of non-renewable resources including energy sources	0 There is no evidence to suggest whether this site is particularly suitable or unsuitable for sustainable design and construction and renewable energy technologies. Effects are expected to be dependent on the development of the masterplan.
	1.3 Limit water consumption to levels supportable by natural processes and storage systems	0 The absolute effect of further development will be to put additional strain on already stretched water resources in the sub-region, therefore possibly leading to lowered groundwater levels and impacts on natural processes. However, no particular constraints to this site in terms of water resources have been identified.
Biodiversity	2.1 Avoid damage to designated sites and protected species	+ Development at this location should not lead to significant impacts on protected species.
	2.2 Maintain and enhance the range and viability of characteristic habitats and species	? Within the site the area of elm woodland is particularly noteworthy, and additional residential development could subject it to further intrusion and disturbance by the public.  The wildflower grass strip (around 5m in depth) parallel to the guided bus way should be retained, as it retains the qualities of the original field margin.  It will be important that further studies are undertaken to ascertain the true value of the grassland and woodland habitats.
	2.3 Improve opportunities for people to access and appreciate wildlife and wild places	0 Public rights of way lead from the site, but it is not clear that there is high quality or biodiversity rich countryside within walking distance.
Landscape, townscape and archaeology	3.1 Avoid damage to areas and sites designated for their historic interest, and protect their settings	? The site is located in an area of high archaeological potential. The County Council would recommend that the site should be subject to a programme of archaeological investigation in advance of development, but would not necessarily object to development at this location.



	<p>3.2 Maintain and enhance the diversity and distinctiveness of landscape and townscape character</p>	<p>-? The nature of the properties on Manor Park facing away from the site gives the area the character of an area of countryside outside the built up area of the village. The site relates more to the surrounding countryside beyond the guided busway route than the developed area of the village.</p> <p>The north western part of the site is heavily treed woodland. Consideration would need to be given to the impact of development on this area, and it would be desirable for them to be retained and excluded from any development.</p> <p>It is considered that this wooded area contributes to the character of this part of the village.</p>
	<p>3.3 Create places, spaces and buildings that work well, wear well and look good</p>	<p>0 Effects will depend on the final masterplan</p>
<p>Climate change and pollution</p>	<p>4.1 Reduce emissions of greenhouse gasses and other pollutants (including air, water, soil, noise, vibration and light)</p>	<p>?+ There is a good bus service into the centre of Cambridge, which will become even better with the opening of the Cambridge Guided Bus service. However, the bus route is currently 600 to 1000m away from the proposed development. This is not an ideal distance, but one that some people should be prepared to walk on a daily basis.</p> <p>Histon is connected to the wider Cambridge cycle network. Once the Cambridgeshire Guided Busway (CGB) is opened in 2009 there will be a new dedicated cycle route travelling alongside the CGB track; this cycle route would be accessible from the boundary of this site, therefore the site would benefit from a high quality segregated cycle route that leads to the Science Park, Arbury Park and northern sections of Cambridge.</p> <p>A car driver mode share of around 60% could be expected. However with the advent of the CGB and its dedicated cycle/pedestrian route it is possible that a lower car driver mode share could be achieved.</p>
	<p>4.2 Minimise waste production and support the recycling of waste products</p>	<p>0 At this stage there is no evidence to suggest how suitable development at this location will be for adopting sustainable waste management practices.</p>
	<p>4.3 Limit or reduce vulnerability to the effects of climate change (including flooding)</p>	<p>+? The site is in Flood Zone 1. A flood risk assessment would need to consider the impact off site, as there are known flooding issues in Histon / Impington).</p>

Healthy communities	5.1 Maintain and enhance human health	-? Due to adjacent land-uses odour pollution may be a serious constraint to development at this location.  Furthermore, due to adjacent land-uses noise pollution may be a serious constraint to development at this location.  Contaminated land is an important consideration that will require investigation
	5.2 Reduce and prevent crime, and reduce the fear of crime	0 There is no evidence to suggest whether this site is particularly suited or unsuited to address issues of crime or fear of crime.
	5.3 Improve the quantity and quality of publicly accessible open space	- The site is currently allocated to be 'developed' as formal open space in the future. Histon and Impington has a poor level of open space provision, and public open space in this location would improve accessibility for this part of the village. There is already good walking access via the existing footpath, and the site is already used informally for recreation. Loss of this allocation would harm the ability to overcome this shortfall.
Inclusive communities	6.1 Improve the quality, range and accessibility of services and facilities (e.g. health, transport, education, training, leisure opportunities)	+ A number of services and facilities are found within a distance of 1–1.5 km in Histon including a supermarket, primary school, post office, doctor's surgery, outdoor sports pitches and formal children's playspace. A secondary school is located less than 2km away.  The development could provide open space, but there is little opportunity for this scale of development to provide other new services or facilities.
	6.2 Redress inequalities related to age, gender, disability, race, faith, location and income	0 It is not thought that there are particular local derivational inequalities that could be addressed in anyway by development at this location.
	6.3 Ensure all groups have access to decent, appropriate and affordable housing	+ This development is located in relatively close proximity to Cambridge, which is the area where there is the greatest existing need for affordable housing (although South Cambridgeshire – the area surrounding Cambridge – also has significant housing needs).
	6.4 Encourage and enable the active involvement of local people in community activities	0 Given the limited impact of this development on the existing scale and character of the settlement, it is not thought that there would be any significant impacts on the existing community.
Economic activity	7.1 Help people gain access to satisfying work appropriate to their skills, potential and place of residence	+ The site is relatively close to the science park (5km) and 6km from the city centre. The site will benefit from access to the guided busway after its completion in 2009 although the nearest stop is far enough away to mean that it may not be easily accessible to all.

	7.2 Support appropriate investment in people, places, communications and other infrastructure	0 There will be no significant effects in terms of this SA Objective
	7.3 Improve the efficiency, competitiveness, vitality and adaptability of the local economy	0 There will be no significant effects in terms of this SA Objective
<p><b>Summary</b></p> <p>This is a greenfield development, but on land that is currently underused, rather than being used for agriculture. However, there is an area of mature woodland onsite and the site is currently used informally for recreational purposes. The open nature (relating well to the countryside) and perceived recreational use means that development of this site would have an adverse impact on the character of this part of the village. Furthermore, the site is currently allocated to be 'developed' as formal open space in the future, and this lost opportunity is a significant consideration.</p> <p>There is a good bus service and access to a high quality cycle network, although the bus stops and other services and facilities in Histon are located a considerable walk away. Therefore, it is unclear whether the location is particularly suited to reducing car-dependency.</p> <p>The compatibility of residential development with the adjoining land uses, particularly with regard to noise and odour, would need to be investigated, and issues may be difficult to overcome.</p> <p><b>Recommendations</b></p> <p>If this location is to go forward for further consideration it will be important to:</p> <ol style="list-style-type: none"> <li>1. It will be important that further studies are undertaken to ascertain the true value of the grassland and woodland habitats</li> <li>2. Ensure that any residual noise and odour constraints can be over-come by mitigation.</li> </ol>		

### Assessment table for Powell's garage, Woollards Lane, Great Shelford

SA Topic	SA Objective	Assessment findings
Land and water resources	1.1 Minimise the irreversible loss of undeveloped land and productive agricultural holdings	+ This is a brownfield site
	1.2 Reduce the use of non-renewable resources including energy sources	0 There is no evidence to suggest whether this site is particularly suitable or unsuitable for sustainable design and construction and renewable energy technologies. Effects are expected to be dependent on the development of the masterplan.
	1.3 Limit water consumption to levels supportable by natural processes and storage systems	0 The absolute effect of further development will be to put additional strain on already stretched water resources in the sub-region, therefore possibly leading to lowered groundwater levels and impacts on natural processes. However, no particular constraints to this site in terms of water resources have been identified.
Biodiversity	2.1 Avoid damage to designated sites and protected species	+ Development would avoid damage to designated sites and protected species
	2.2 Maintain and enhance the range and viability of characteristic habitats and species	+? Due to the nature of the site the impact on biodiversity is likely to be limited. However, even Brownfield sites have the potential biodiversity interest so this might need to be further explored
	2.3 Improve opportunities for people to access and appreciate wildlife and wild places	0 This small site is within the centre of the village so there is not direct access to the countryside.
Landscape, townscape and archaeology	3.1 Avoid damage to areas and sites designated for their historic interest, and protect their settings	? The site is located in the historic core of the village to the north east of the medieval parish church of St Mary. The Conservation Area Appraisal identifies the site as a positive building, and a focal building. However, this building has been insensitively added to in its current garage use and its historic lines and character largely obscured, although it could potentially be restored. It is surrounded by large areas of surface car parking. Much will depend on the quality and design of what replaces it in terms of the net change on the character and appearance of the conservation area.
	3.2 Maintain and enhance the diversity and distinctiveness of landscape and townscape character	? Although the site is located in the historic core of the village this is a very modest redevelopment of an existing site and would not necessarily have implications for overall settlement character.

	3.3 Create places, spaces and buildings that work well, wear well and look good	0 Effects will depend on the final masterplan
Climate change and pollution	4.1 Reduce emissions of greenhouse gasses and other pollutants (including air, water, soil, noise, vibration and light)	+ The site has good access to local services and facilities being located on the edge of the village centre. It also has good access to limited public transport and is just over a 5 minute walk to a regular bus service to Cambridge. However, it is still likely to have a relatively high modal share for the private car unless the 30 minute journey times into the city by public transport can be reduced. The introduction of the southern section of the Guided Busway may however encourage the use of sustainable modes.
	4.2 Minimise waste production and support the recycling of waste products	0 At this stage there is no evidence to suggest how suitable development at this location will be for adopting sustainable waste management practices.
	4.3 Limit or reduce vulnerability to the effects of climate change (including flooding)	+ Flood risk is not expected to be a major issue, especially given the relatively small size of the proposed development.
Healthy communities	5.1 Maintain and enhance human health	0 Existing public outdoor sport facilities and children's formal playspace are located within a short walk distance from the proposed site.  Existing garage uses would require investigation for contamination and potentially remediation.
	5.2 Reduce and prevent crime, and reduce the fear of crime	0 There is no evidence to suggest whether the site will be particularly suited or unsuited to prevent crime or reduce fear of crime.
	5.3 Improve the quantity and quality of publicly accessible open space	0 There is easy access to public outdoor sport facilities and children's playspace.
Inclusive communities	6.1 Improve the quality, range and accessibility of services and facilities (e.g. health, transport, education, training, leisure opportunities)	+ There is limited potential for additional facilities on this relatively small village centre site. However, since it is located in the village centre people would benefit by access to existing services and facilities which are located only a short walk distance from the site. In addition Cambridge city centre is relatively easily accessible by a regular bus service.
	6.2 Redress inequalities related to age, gender, disability, race, faith, location and income	0 It is not thought that there are particular local derivational inequalities that could be addressed in anyway by development at this location.

	6.3 Ensure all groups have access to decent, appropriate and affordable housing	+ This development is located in relatively close proximity to Cambridge, which is the area where there is the greatest existing need for affordable housing (although South Cambridgeshire – the area surrounding Cambridge – also has significant housing needs).
	6.4 Encourage and enable the active involvement of local people in community activities	0 The scale of this development implies that there would be no significant effect on the existing community.
Economic activity	7.1 Help people gain access to satisfying work appropriate to their skills, potential and place of residence	? The site is relatively well situated in terms of access to major employment locations at Cambridge City Centre and Addenbroses.  However the development would result in the loss of a small existing commercial site.
	7.2 Support appropriate investment in people, places, communications and other infrastructure	0 There will be no significant effects in terms of this SA Objective
	7.3 Improve the efficiency, competitiveness, vitality and adaptability of the local economy	0 Would result in loss of an existing commercial site although employee numbers are likely to be relatively low.
<p><b>Summary</b> This is a brownfield site with no obvious biodiversity constraints. However, there are important heritage and townscape considerations as a result of the site being located within a conservation area.</p> <p>There is limited potential for additional facilities on this relatively small village centre site. However, since it is located in the village centre people would benefit by access to existing services and facilities which are located only a short walk distance from the site. In addition Cambridge City Centre is relatively easily accessible by a regular bus service. Major employment locations at Cambridge City Centre and Addenbroke are in relatively close proximity. However the development would result in the loss of a small existing commercial site.</p> <p><b>Recommendations</b> If this location is to go forward for further consideration it will be important to:</p> <ol style="list-style-type: none"> <li>1. Demonstrate that the impact on the Conservation Area and the historic character of this part of the village could be addressed through an appropriate design.</li> </ol>		

### Assessment table for the Ida Darwin Hospital, Fulbourn

SA Topic	SA Objective	Assessment findings
Land and water resources	1.1 Minimise the irreversible loss of undeveloped land and productive agricultural holdings	+ The site contains a series of buildings set in landscaped grounds.
	1.2 Reduce the use of non-renewable resources including energy sources	0 There is no evidence to suggest whether this site is particularly suitable or unsuitable for sustainable design and construction and renewable energy technologies. Effects are expected to be dependent on the development of the masterplan.
	1.3 Limit water consumption to levels supportable by natural processes and storage systems	-? The absolute effect of further development will be to put additional strain on already stretched water resources in the sub-region, therefore possibly leading to lowered groundwater levels and impacts on natural processes.  Any development would have to be sympathetic to the underlying geology as the site is located above a major aquifer (Source Protection Zones 1, 2 and 3). It will be important that groundwater recharge is not affected and that there is no risk of pollution to this important aquifer.
Biodiversity	2.1 Avoid damage to designated sites and protected species	+ Development would avoid damage to designated sites and protected species
	2.2 Maintain and enhance the range and viability of characteristic habitats and species	+ There are no features on-site that are easily identifiable as being of biodiversity importance, although as the site is landscaped and relatively diverse it will be important that a detailed survey identifies any important habitats that may have established themselves.
	2.3 Improve opportunities for people to access and appreciate wildlife and wild places	+ The relocation of some of the existing built footprint to Fulbourn Hospital provides the opportunity to create a new area of open space with a countryside character on the western part of the site.  To the east of Fulbourn is a range of important designated and non-designated habitat including Fulbourn Fen SSSI and Great Wilbraham Common SSSI.

Landscape, townscape and archaeology	3.1 Avoid damage to areas and sites designated for their historic interest, and protect their settings	<p>+</p> <p>The site is located to the immediate south of a Roman settlement considered to be of national importance and subject to statutory designation. Further evidence of Iron Age and Roman settlement is known to the east of the Scheduled Monument. However, given the fact that the site is already developed, there is no indication that re-development would have a significant effect on local historic or archaeological interest.</p>
	3.2 Maintain and enhance the diversity and distinctiveness of landscape and townscape character	<p>?</p> <p>The site is located in the Green Belt, but is a Major Developed Site (MDS) in the Green Belt, meaning that development would not necessarily impact upon the character of the Green Belt assuming it was appropriately designed and does not increase the built footprint.</p> <p>However, the Ida Darwin Hospital site, which adjoins the western end of Fulbourn village is developed at a relatively low density in landscaped grounds giving it a significantly different character to the village and justifying its location in the Green Belt. Conservation Areas have been designated to the east and west of the site, and so it will be important that development is sympathetic to the character of the village.</p> <p>Development would need to be carefully designed to protect the setting of Cambridge but also has potential to enhance the separation of Cambridge with the necklace village of Fulbourn by consolidation of the built footprint to the eastern part of the site leaving the western part of the site open.</p>
	3.3 Create places, spaces and buildings that work well, wear well and look good	<p>0</p> <p>Effects will depend on the final masterplan</p>
Climate change and pollution	4.1 Reduce emissions of greenhouse gasses and other pollutants (including air, water, soil, noise, vibration and light)	<p>+?</p> <p>The site is located relatively close to Cambridge and other employment sites and offers the potential for sustainable travel by both bus and cycle. However, bus journey times into the city may make the bus a relatively unattractive option as a mode of travel.</p>
	4.2 Minimise waste production and support the recycling of waste products	<p>0</p> <p>At this stage there is no evidence to suggest how suitable development at this location will be for adopting sustainable waste management practices.</p>
	4.3 Limit or reduce vulnerability to the effects of climate change (including flooding)	<p>?</p> <p>There is a high water table in the general area. There have been serious flooding incidents in St Thomas Road to the West. A pumped land drainage system was recently installed to prevent flooding of the area from a combination of high ground water and heavy periods of rainfall.</p>



Healthy communities	5.1 Maintain and enhance human health	<p>0+</p> <p>This site is in a location that could encourage walking and cycling.</p> <p>Regarding noise, a single railway is adjacent to the north of the site. Noise issues need to be assessed, but could most likely be mitigated with careful design and positioning of the buildings.</p> <p>In addition, due to the historical use as a hospital contaminated land is a material consideration that will require investigation and remedial action as necessary</p>
	5.2 Reduce and prevent crime, and reduce the fear of crime	<p>0</p> <p>There is no evidence on whether the site will be particularly suited or unsuited to prevent crime or reduce fear of crime.</p>
	5.3 Improve the quantity and quality of publicly accessible open space	<p>+</p> <p>The relocation of some of the existing built footprint to Fulbourn Hospital provides the opportunity to create a new area of open space with a countryside character on the western part of the site and a greater extent of separation with Fulbourn Hospital, which could potentially provide public access.</p>
Inclusive communities	6.1 Improve the quality, range and accessibility of services and facilities (e.g. health, transport, education, training, leisure opportunities)	<p>?</p> <p>The site has reasonable accessibility on foot to local services and facilities being around 15 minutes away.</p> <p>The site is located within 7km from Cambridge city centre, however bus service is limited and takes up to 45 minutes to reach the city centre from the proposed site. Therefore it is more difficult to access higher order services than might be the case for other locations around the edge of Cambridge. Transport improvements could come forward, although would primarily increase frequency, which is also important.</p> <p>Regarding school access, development could be accommodated through the expansion of the existing primary school, and the expansion of Bottisham Village College.</p>
	6.2 Redress inequalities related to age, gender, disability, race, faith, location and income	<p>0</p> <p>It is not thought that there are particular local derivational inequalities that could be addressed in anyway by development at this location.</p>
	6.3 Ensure all groups have access to decent, appropriate and affordable housing	<p>+</p> <p>This development is located in relatively close proximity to Cambridge, which is the area where there is the greatest existing need for affordable housing (although South Cambridgeshire – the area surrounding Cambridge – also has significant housing needs).</p>
	6.4 Encourage and enable the active involvement of local people in community activities	<p>0</p> <p>This is a redevelopment of and existing developed site and lies outside the village framework, and so it is not thought that there would be negative effects on the scale or character of the village to the detriment of the community.</p>

Economic activity	7.1 Help people gain access to satisfying work appropriate to their skills, potential and place of residence	+ The site is located relatively close to Cambridge City Centre (7km) and Addenbrookes (5km). There is also access to employment more locally at Capital Park.
	7.2 Support appropriate investment in people, places, communications and other infrastructure	0 There will be no significant effects in terms of this SA Objective
	7.3 Improve the efficiency, competitiveness, vitality and adaptability of the local economy	0+ This development appears to support the continued viability of the adjacent Fulbourn Hospital, although this effect is not certain and it is not clear how significant the hospital is in terms of the local economy

**Summary**

This is a brownfield development at a location with no major biodiversity constraints. However, the Fulbourn Hospital site is designated as a Conservation Area and is a well wooded and attractive landscape. The site is in the Green Belt, but it is likely that the Green Belt function can be retained. The main environmental constraint is the sensitivity of the underlying aquifer, which could potentially be put at risk by inappropriate development. There is potential to make use of the existing landscaped areas and create publicly accessible open space, and there is also biodiversity rich countryside to the East of Fulbourn.

The site is located relatively close to Cambridge and employment sites and offers the potential for sustainable travel by both bus and cycle, but is likely to still have a relatively high modal share for the private car unless journey times into the city by public transport can be reduced. The site has reasonable accessibility on foot to local services and facilities being around 15 minutes away.

**Recommendations**

If this location is to go forward for further consideration it will be important to:

1. Ensure groundwater recharge is not affected and that there is no risk of pollution to this important aquifer.
2. Ensure that noise issues area assessed and mitigated.
3. Ensure development is sympathetic to the adjacent Conservation Areas and the SAM as well as the setting of, Cambridge and Fulbourn more generally.
4. Seek to improve access to high quality public transport

### Assessment table for Land between Teversham Road and Cow Lane

SA Topic	SA Objective	Assessment findings
Land and water resources	1.1 Minimise the irreversible loss of undeveloped land and productive agricultural holdings	- The land consists of two open fields, divided by a substantial hedge which runs north to south through the middle of the site. It is predominantly Grade 2.
	1.2 Reduce the use of non-renewable resources including energy sources	0 There is no evidence to suggest whether this site is particularly suitable or unsuitable for sustainable design and construction and renewable energy technologies. Effects are expected to be dependent on the development of the masterplan.
	1.3 Limit water consumption to levels supportable by natural processes and storage systems	-? The absolute effect of further development will be to put additional strain on already stretched water resources in the sub-region, therefore possibly leading to lowered groundwater levels and impacts on natural processes.  Any development would have to be sympathetic to the underlying geology as the site is located above a major aquifer (Source Protection Zones 1). It will be important that groundwater recharge is not affected and that there is no risk of pollution to this important aquifer.
Biodiversity	2.1 Avoid damage to designated sites and protected species	+ Development would avoid damage to designated sites and protected species
	2.2 Maintain and enhance the range and viability of characteristic habitats and species	?- A chalk stream is in the vicinity of the site, which feeds into Wilbraham Fen, a SSSI (approximately 3km to the north). Any pollution may lead to the degradation of this important site.
	2.3 Improve opportunities for people to access and appreciate wildlife and wild places	+ Countryside access to the north is limited by the presence of the railway line, although there is biodiversity rich countryside accessible to the east of Fulbourn.
Landscape, townscape and archaeology	3.1 Avoid damage to areas and sites designated for their historic interest, and protect their settings	?- The site is located in an area with high potential for surviving evidence of the Roman settlement of the area. However, the County Council would not object to the allocation of this site for redevelopment, subject to appropriate investigation..  Conservation Area adjoins to the south.

	3.2 Maintain and enhance the diversity and distinctiveness of landscape and townscape character	-? Development would extend the built up area of the village north, towards the railway line. It is possible that the current soft village edge would be lost, which is an important element of the rural setting of the northern part of the village and also the setting of the Conservation Area.  However, the site is relatively enclosed and so despite adjoining the Green Belt the impact of development on the wider landscape would be limited.
	3.3 Create places, spaces and buildings that work well, wear well and look good	0 Effects will depend on the final masterplan
Climate change and pollution	4.1 Reduce emissions of greenhouse gasses and other pollutants (including air, water, soil, noise, vibration and light)	? The site is within cycling distance of Cambridge and has an off-road route and public transport. However journey time to Cambridge remains an issue with only 3 buses per hour and 50 minutes to reach the city centre from the centre of Fulbourn. Therefore the expectation is that private car use might still be high unless there is an increase in the quality of public transport. It is not likely that this development in isolation would lead to such improvements.
	4.2 Minimise waste production and support the recycling of waste products	0 At this stage there is no evidence to suggest how suitable development at this location will be for adopting sustainable waste management practices.
	4.3 Limit or reduce vulnerability to the effects of climate change (including flooding)	? There is a high water table in the general area. There have been serious flooding incidents in St Thomas Road to the West. A pumped land drainage system was recently installed to prevent flooding of the area from a combination of high ground water and heavy periods of rainfall. .
Healthy communities	5.1 Maintain and enhance human health	- The site is within cycling distance of Cambridge and so could encourage cycling.  The North West part of the site is in close proximity to some industrial units and there is concern about placing noise sensitive receptors such as residential uses into an existing noisy environment.
	5.2 Reduce and prevent crime, and reduce the fear of crime	0 There is no evidence on whether the site will be particularly suited or unsuited to prevent crime or reduce fear of crime.
	5.3 Improve the quantity and quality of publicly accessible open space	0+ The site is something of a green lung for Fulbourn, however, alternative open space is within walking distance and there is potential for local open space provision as part of the development.

Inclusive communities	6.1 Improve the quality, range and accessibility of services and facilities (e.g. health, transport, education, training, leisure opportunities)	<p>+</p> <p>The scale of the site would not support any significant enhancement of local services and facilities. The development would not therefore in itself provide a sustainable mixed development and would rely on existing local services and facilities, for which it would provide extra patronage.</p> <p>The site has reasonable level of accessibility to local services and facilities with food shop and post office being within a walking distance from the site (720m and 805m respectively from the nearest point of the site) and the primary school within a 15 minute walk (about 1km from the nearest point).</p> <p>It is within cycling distance of Cambridge where people can access higher order services, and has an off-road route and public transport although the latter is not high quality.</p>
	6.2 Redress inequalities related to age, gender, disability, race, faith, location and income	<p>0</p> <p>It is not thought that there are particular local derivational inequalities that could be addressed in anyway by development at this location.</p>
	6.3 Ensure all groups have access to decent, appropriate and affordable housing	<p>+</p> <p>This development is located in relatively close proximity to Cambridge, which is the area where there is the greatest existing need for affordable housing (although South Cambridgeshire – the area surrounding Cambridge – also has significant housing needs).</p>
	6.4 Encourage and enable the active involvement of local people in community activities	<p>0</p> <p>It is not thought that there would be negative effects on the scale or character of the village to the detriment of the community.</p>
Economic activity	7.1 Help people gain access to satisfying work appropriate to their skills, potential and place of residence	<p>+</p> <p>The site is located 7.5km from Cambridge City Centre and 5.5km from Addenbroses. There is also access to employment more locally at Capital Park.</p>
	7.2 Support appropriate investment in people, places, communications and other infrastructure	<p>0</p> <p>There will be no significant effects in terms of this SA Objective</p>
	7.3 Improve the efficiency, competitiveness, vitality and adaptability of the local economy	<p>0</p> <p>There will be no significant effects in terms of this SA Objective</p>
<p><b>Summary</b></p> <p>There are important landscape and townscape considerations. Development would extend the built up area of the village north, towards the railway line. Development of part of the site, on the field currently fronting Teversham Road, would have minimal impact as the field is relatively well contained and has little impact on the character of the Conservation Area. However, development of the larger field to the rear of</p>		

the Poor Well *would* have an impact on the character of the Conservation Area.

Furthermore, there are serious concerns regarding noise pollution from adjacent land-uses and the site is located in the Source Protection Zone above a sensitive aquifer, which could potentially be put at risk by inappropriate development.

The site is located relatively close to Cambridge and other employment sites and offers the potential for sustainable travel by both bus and cycle, but is likely to still have a relatively high modal share for the private car unless journey times into the city by public transport can be reduced. The site has reasonable accessibility on foot to local services and facilities being around 15 minutes away.

### **Recommendations**

If this location is to go forward for further consideration it will be important to:

1. Ensure groundwater recharge is not affected and that there is no risk of pollution to this important aquifer.
2. Ensure that development is sensitive to the setting of the conservation area
3. Seek to improve access to high quality public transport

### Assessment table for Land West of Cambourne

SA Topic	SA Objective	Assessment findings
Land and water resources	1.1 Minimise the irreversible loss of undeveloped land and productive agricultural holdings	- This land is predominantly Grade 2 agricultural land.
	1.2 Reduce the use of non-renewable resources including energy sources	0 There is no available evidence to suggest that this site is particularly suitable or unsuitable for the incorporation of sustainable design and construction and renewable energy technologies. Effects will be dependent on the development of the masterplan.
	1.3 Limit water consumption to levels supportable by natural processes and storage systems	0? The absolute effect of further development will be to put additional strain on already stretched water resources in the sub-region, therefore possibly leading to lowered groundwater levels and impacts on natural processes.  In relative terms, there is little evidence to suggest that development at this location in South Cambs would lead to a risk of particular effects on water resources, either by water abstraction or water pollution (this is not a source protection zone).  However, the Environment Agency has expressed concern with regard to the impacts on the recipient watercourse from the Sewage Treatment Works (STW) that would be the likely recipient of foul water drainage from the proposed development.  Any development at this location would be required to demonstrate that there was enough capacity within the recipient foul water drainage system, or that the system could be upgraded in order to facilitate any proposed growth.
Biodiversity	2.1 Avoid damage to designated sites and protected species	0? About 1.5km north of the site is Elsworth Wood SSSI, a small ancient woodland that is in 'unfavourable recovering' condition. The wood is adjacent to a footpath, which connects it to the proposed site, although it thought that this wood is not openly accessible to the public. Nonetheless, it will be important to consider the potential for impacts on the SSSI.

	2.2 Maintain and enhance the range and viability of characteristic habitats and species	<p>0-</p> <p>Given the relatively unremarkable nature of the habitats present, development at this location would not necessarily lead to populations of important species at the landscape scale being put under pressure.</p> <p>However, some specific features of relative biodiversity interest are present on-site, and it will be important that these are incorporated into the masterplan in such a way that allows them to retain their functional value as much as possible.</p>
	2.3 Improve opportunities for people to access and appreciate wildlife and wild places	<p>+?</p> <p>This site has good access to the countryside via public footpath.</p>
Landscape, townscape and archaeology	3.1 Avoid damage to areas and sites designated for their historic interest, and protect their settings	<p>-?</p> <p>The site is located in a landscape of high archaeological potential. The County Council would need further information before they could provide detailed advice on the suitability of the site for development, but on the basis of current information would object to the allocation of this site, as development could have a severe detrimental impact on the archaeological landscape</p>
	3.2 Maintain and enhance the diversity and distinctiveness of landscape and townscape character	<p>-</p> <p>As part of a 1992 Inquiry into the location of a new settlement at this location, the Planning Inspector made a number of comments on the importance of this landscape and concluded that “although the quality of the landscape within the site itself is not high, it is an essential part of a wide and open landscape of considerable character.”</p> <p>The development of Cambourne has already impacted on this locally important landscape, and so the cumulative effect of the further proposed development would be significant.</p> <p>The existing landscape in the vicinity of the proposed development has also taken on a new value since the development of Cambourne as it contributes to the setting of this new community. It is safe to say that the rural setting of Cambourne is an important element in creating a distinctive sense of community.</p> <p>Finally, the site does relate relatively well to the existing form of Cambourne and could provide an extension to the village that would be perceived as such visually and functionally. However, the development would take the overall scale of Cambourne to over 5000 homes, which is well above the number of homes initially thought necessary to achieve the vision for the new community. The character of Cambourne would therefore be transformed considerably more towards that of a small town.</p>



	3.3 Create places, spaces and buildings that work well, wear well and look good	0 Effects will be dependent on the final masterplan
Climate change and pollution	4.1 Reduce emissions of greenhouse gasses and other pollutants (including air, water, soil, noise, vibration and light)	- This site is not particularly well located in terms of minimising car dependency. It is located further from Cambridge City Centre and key employment sites than any of the other proposed locations for development (although there is likely to be considerable employment growth in Cambourne).  A bus journey from the site to Cambridge would take about 45 minutes and so, given that this site is located adjacent to the major road network, a modal shift away from reliance on the car is not likely.  A neighbourhood centre in West Cambourne is proposed. However, its deliverability is questionable. If it did not come forward there would be a reliance on the use of services and facilities in the existing village centre, which is a relatively long walk away.
	4.2 Minimise waste production and support the recycling of waste products	0 At this stage there is no evidence to suggest how suitable development at this location will be for adopting sustainable waste management practices.
	4.3 Limit or reduce vulnerability to the effects of climate change (including flooding)	? It would appear that this site is unlikely to be at risk of flooding now or in the future. It will be important that further studies establish the off-site impacts of increased surface water run-off at this location as a result of development.
Healthy communities	5.1 Maintain and enhance human health	0 This location has good access to existing playing fields and there are also children's play facilities located less than 1 km away in Cambourne. There is good access to open countryside via public footpaths. However, as highlighted by the assessment against SA Objective 4.1, this site is not likely to encourage people to walk and cycle to access services and facilities.  The north of the site is bounded by the A428, which is a source of both air pollution and noise pollution. Both of these forms of pollution are likely to increase from their baseline levels as a result of future development at this site and elsewhere in the sub-region. However, given adequate mitigation (particularly buffering between the road and housing), it is unlikely that there will be any significant effects on human health.
	5.2 Reduce and prevent crime, and reduce the fear of crime	0 There is no evidence available to suggest whether this location will be particularly suited or unsuited to preventing crime or the fear of crime. Much will depend on the final masterplan.

	5.3 Improve the quantity and quality of publicly accessible open space	+ This location has good access to existing playing fields and there is also good access to open countryside via public footpaths. Developers propose to deliver new open space.
Inclusive communities	6.1 Improve the quality, range and accessibility of services and facilities (e.g. health, transport, education, training, leisure opportunities)	- A number of services and facilities are located in Cambourne, although none of these are within 1km of the edge of the site that borders Cambourne. The nearest Post Office is some way away (3.5km)  This site is located approximately 14km from Cambridge City Centre, which would be the most likely destination to access higher order services.  The site is some distance from the nearest secondary school (about 12km). The County Council has advised that there is a need for a secondary school to serve Cambourne based on current planned growth which is being explored, and that provision is not dependent on additional growth. However, additional development would add to the demand for secondary places and would also offer the opportunity for identifying a site for a secondary school.
	6.2 Redress inequalities related to age, gender, disability, race, faith, location and income	0 It is not thought that there are particular local deprivational inequalities that could be addressed in anyway by development at this location.
	6.3 Ensure all groups have access to decent, appropriate and affordable housing	+ Development at Cambourne would contribute towards meeting the housing needs that exist in South Cambridgeshire District. However, Cambourne is some distance away from Cambridge, where the greatest housing needs can be found.
	6.4 Encourage and enable the active involvement of local people in community activities	?- Much will depend on the final masterplan. However, it is important to consider the impact that further expansion of Cambourne will have the sense of place and sense of community felt by those existing residents of Cambourne.
Economic activity	7.1 Help people gain access to satisfying work appropriate to their skills, potential and place of residence	? There is a considerable amount of employment development in Cambourne, which will be easily accessible from the site and the objector proposes an extension to the Business Park. However, this site is not well located in terms of the major employment locations at the Cambridge Science Park, Addenbokes (both 16km away ) and Cambridge City Centre (14km / 45 minute bus journey away).  There is a small amount of existing employment on-site, and so it will be important that negative impacts on the viability of this employment activity are avoided where possible.

	7.2 Support appropriate investment in people, places, communications and other infrastructure	0 There will be no significant effects in terms of this SA Objective.
	7.3 Improve the efficiency, competitiveness, vitality and adaptability of the local economy	+ There is still a need to attract business to the land and premises available at Cambourne, and so in this sense it might be seen as beneficial to locate more people in close proximity to this employment land.
<p><b>Summary</b></p> <p>This is a greenfield site on good quality agricultural land, albeit land without any major environmental constraints to development. The land is, however, important in terms of the landscape setting of Cambourne. Furthermore, the archaeological potential of the site is a constraint to be addressed.</p> <p>Cambourne is the least sustainable of the Rural Centres in terms of location relative to Cambridge and quality of public transport. Reliance on the private car to access higher order services and facilities as well as major employment locations would most likely be relatively high, and there are concerns about the potential to bring forward lower order services and facilities commensurate with the level of development within walking / cycling distance. The location of this site does, however, mean that there is good access to open and biodiversity rich countryside as well as other open space and play areas.</p> <p><b>Recommendations</b></p> <p>If this location is to go forward for further consideration it will be important to:</p> <ol style="list-style-type: none"> <li>1. Consider foul water drainage constraints.</li> <li>2. Consider the potential for impacts on the SSSI.</li> <li>3. Incorporate existing biodiversity features into the masterplan in such a way that allows them to retain their functional value.</li> <li>4. Establish the off-site impacts of increased surface water run-off at this location.</li> <li>5. Ensure that a good range of local services and facilities are accessible within walking distance.</li> <li>6. Consider how effects on the community / sense of place in Cambourne might be mitigated.</li> <li>7. Give consideration to the high archaeological potential of the site.</li> </ol>		

### Assessment table for Land North of A428, Cambourne

SA Topic	SA Objective	Assessment findings
Land and water resources	1.1 Minimise the irreversible loss of undeveloped land and productive agricultural holdings	- The land is mainly agricultural land (Grade 2 and small part Grade 3)
	1.2 Reduce the use of non-renewable resources including energy sources	0 There is no evidence to suggest whether this site is particularly suitable or unsuitable for sustainable design and construction and renewable energy technologies. Effects are expected to be dependent on the development of the masterplan.
	1.3 Limit water consumption to levels supportable by natural processes and storage systems	0? The absolute effect of further development will be to put additional strain on already stretched water resources in the sub-region, therefore possibly leading to lowered groundwater levels and impacts on natural processes.  In relative terms, there is little evidence to suggest that development at this location in South Cambs would lead to a risk of particular effects on water resources, either by water abstraction or water pollution (this is not a source protection zone).  However, the Environment Agency has expressed concern with regard to the impacts on the recipient watercourse from the Sewage Treatment Works (STW) which would be the likely recipient of foul water drainage from development in this area.  Any development at this location would be required to demonstrate that there was enough capacity within the recipient foul water drainage system, or that the system could be upgraded in order to facilitate any proposed growth.

Biodiversity	2.1 Avoid damage to designated sites and protected species	<p>-?</p> <p>Within the site but outside the area proposed for development is Elsworth Wood SSSI, a small ancient woodland. This is an important consideration, and until it is established what potential there is to impact directly or indirectly on the SSSI this must be considered a significant negative effect.</p> <p>It will be important to consider the full range of potential indirect effects that could impact on the ecological health of the SSSI, including recreation, mis-use, pollution and impacts on non-designated habitats in the vicinity of the SSSI that might in some ways support its ecological functioning.</p> <p>Furthermore, two County Wildlife Sites, Knapwell Wood and Knapwell Roadside Verge, adjoin the area proposed for built development. Brockley End Meadow adjoins the north of the larger objection site.</p> <p>As part of a previous inquiry about the potential development of a site known as Belham Hill, which comprised the southern part of the current site, the Planning Inspector had objection to the development of this site because it was too close to very important areas and wildlife interest.</p> <p>Despite these features, an ecological appraisal has concluded that the master plan provided by the developer is predicted to have relatively few impacts on nature conservation interest.</p>
	2.2 Maintain and enhance the range and viability of characteristic habitats and species	<p>?</p> <p>Aside from a number of copses / small woodlands (which it is assumed will not be directly impacted upon) the site is largely agricultural fields, separated by hedgerows. The area to be built upon is in the eastern part of the site, which comprises mainly fields in agricultural use.</p> <p>There is clearly a need for further studies to ascertain the relative importance of this site in the wider landscape.</p>
	2.3 Improve opportunities for people to access and appreciate wildlife and wild places	<p>+</p> <p>The site has good access to the countryside and biodiversity rich greenspace. Proposals also include plans for the provision of new public countryside open space.</p>

Landscape, townscape and archaeology	3.1 Avoid damage to areas and sites designated for their historic interest, and protect their settings	-? The site is located in a landscape of high archaeological potential. The County Council would need further information before they could provide detailed advice on the suitability of the site for development, but on the basis of current information would object to the allocation of this site, as development could have a severe detrimental impact on the archaeological landscape.  A listed building is located on the southern boundary of the site. Effects will be dependent on the development of the masterplan, but it is expected that suitable mitigation could be put in place
	3.2 Maintain and enhance the diversity and distinctiveness of landscape and townscape character	- The site is an area of attractive landscape with a pronounced landform of rolling hills and small valleys, mature hedgerows, copses and larger areas of mature woodland. Visually, development of this site would be detrimental to the landscape generally and in particular to the setting of these woodlands and the village of Knapwell due to the local topography with high land to the north offering long views across the area.  The site reads in the landscape as part of the wider countryside and has no identifiable visual relationship with Cambourne, and if developed would clearly have a fundamental impact on the character of Cambourne.
	3.3 Create places, spaces and buildings that work well, wear well and look good	0 Effects will depend on the final masterplan
Climate change and pollution	4.1 Reduce emissions of greenhouse gasses and other pollutants (including air, water, soil, noise, vibration and light)	- Regarding car usage, the majority of people are expected to drive to work. A modal shift away from high levels of car dependency is not likely given the good access to the road network and the distance to Cambridge and other key employment sites, services and facilities.  The A428 presents a physical and physiological barrier, impeding efforts to encourage walking or cycling between this site and Cambourne. However, the objector is proposing new employment and a new centre with community facilities as part of the development. Furthermore, the objector is proposing footpath and cycle links to the existing parts of Cambourne, although it is questionable how feasible and attractive this route would be.
	4.2 Minimise waste production and support the recycling of waste products	0 At this stage there is no evidence to suggest how suitable development at this location will be for adopting sustainable waste management practices.

	4.3 Limit or reduce vulnerability to the effects of climate change (including flooding)	? The site is not expected to be at risk of flooding. However, there is concern about contributing to / increasing flood risk in surrounding areas, which would need to be addressed in any Flood Risk Assessment.
Healthy communities	5.1 Maintain and enhance human health	0 The distance and the separation from the existing village and the main village centre could be a significant barrier to walking and cycling. It is also a significant distance from Cambridge.  The site is bounded by the A428, which is a source of both air pollution and noise pollution. Both of these forms of pollution are likely to increase from their baseline levels as a result of future development at this site and elsewhere in the sub-region. However, given adequate mitigation (particularly buffering between the road and housing), it is unlikely that there will be any significant effects on human health.
	5.2 Reduce and prevent crime, and reduce the fear of crime	0 There is no evidence available to suggest whether this location will be particularly suited or unsuited to preventing crime or the fear of crime. Much will depend on the final masterplan.
	5.3 Improve the quantity and quality of publicly accessible open space	+ The site is larger than the proposed development and therefore there is potential for the incorporation of open space. The location also has good access to countryside via public footpaths and bridleways

<p>Inclusive communities</p>	<p>6.1 Improve the quality, range and accessibility of services and facilities (e.g. health, transport, education, training, leisure opportunities)</p>	<p>-</p> <p>The site is 13 km away from Cambridge City Centre which would be the most likely destination to access higher order services.</p> <p>A number of services and facilities are located in Cambourne but most of them are a considerable walk away from the site and in addition they are separated by the dual carriageway A428.</p> <p>A new employment and a new centre with community facilities is proposed as part of the development. This will be important given the difficulty in accessing Cambourne centre by walking and cycling.</p> <p>The physical scope to enhance or provide new facilities within the existing village centre is limited and therefore there is limited potential to provide higher order services and facilities that would help make Cambourne more inherently sustainable.</p> <p>However, new facilities provided in the new development would not be very accessible from the existing village. It could therefore be considered that a new centre would compromise the overall relative sustainability of Cambourne in terms of provision of adequate services and facilities in a central accessible location.</p> <p>Furthermore, new facilities in the objection site would need to be of a level that would not compromise the viability of the existing centre that is now getting established.</p> <p>The nearest secondary school is 11 km. The County Council has advised that there is a need for a secondary school to serve Cambourne based on current planned growth which is being explored, and that provision is not dependent on additional growth. However, additional development would add to the demand for secondary places and would also offer the opportunity for identifying a site for a secondary school.</p>
	<p>6.2 Redress inequalities related to age, gender, disability, race, faith, location and income</p>	<p>0</p> <p>It is not thought that there are particular local deprivational inequalities that could be addressed in anyway by development at this location.</p>
	<p>6.3 Ensure all groups have access to decent, appropriate and affordable housing</p>	<p>+</p> <p>Development at Cambourne would contribute towards meeting the housing needs that exist in South Cambridgeshire District. However, Cambourne is some distance away from Cambridge, where the greatest housing needs can be found.</p>



	6.4 Encourage and enable the active involvement of local people in community activities	?- Much will depend on the final masterplan. However, the overall scale of Cambourne would increase to that of a small town.. Therefore, it is important to consider the impact that further expansion of Cambourne will have the sense of place and sense of community felt by those existing residents of Cambourne.
Economic activity	7.1 Help people gain access to satisfying work appropriate to their skills, potential and place of residence	- There is a considerable amount of employment development in Cambourne, which will be easily accessible from the site. However, this site is not well located in terms of the major employment locations at the Cambridge Science Park (14.5km), Addenbokes (17km away ) and Cambridge City Centre (13km / 45 minute bus journey away).
	7.2 Support appropriate investment in people, places, communications and other infrastructure	0 There will be no significant effects in terms of this SA Objective.
	7.3 Improve the efficiency, competitiveness, vitality and adaptability of the local economy	0 There will be no significant effects in terms of this SA Objective

**Summary**

This is a greenfield site on good quality agricultural land, with significant landscape and ecological constraints.

Cambourne is the least sustainable of the Rural Centres in terms of location relative to Cambridge and quality of public transport. Reliance on the private car to access higher order services and facilities as well as major employment locations would most likely be relatively high, and there are concerns about the potential to bring forward lower order services and facilities commensurate with the level of development within walking / cycling distance. The location of this site does, however, mean that there is good access to open and biodiversity rich countryside as well as other open space and play areas.

Development north of the A428 would not relate well to the existing form of Cambourne and would not provide a sustainable extension to the village in the sense that it is unlikely that walking or cycling will be an attractive form of movement between the two centres. There are a number of questions regarding whether provision of a new centre will compliment the services and facilities available at Cambourne. The location of this development does, however, mean that there is good access to open and biodiversity rich countryside.

**Recommendations**

If this location is to go forward for further consideration it will be important to:

1. Consider foul water drainage constraints.
2. Consider the potential for impacts on the SSSI.
3. Incorporate existing biodiversity features into the masterplan in such a way that allows them to retain their functional value.
4. Establish the off-site impacts of increased surface water run-off at this location.
5. Ensure that a good range of local services and facilities are accessible within walking

distance.

6. Consider effects on the sustainability of the existing village centre at Cambourne
7. Consider how effects on the community / sense of place in Cambourne might be mitigated.
8. Give consideration to the high archaeological potential of the site.

### Assessment table for Bourne Airfield (1), Cambourne

SA Topic	SA Objective	Assessment findings
Land and water resources	1.1 Minimise the irreversible loss of undeveloped land and productive agricultural holdings	? The site comprises a mix of uses and features, including runways, buildings and open land, much of which appears to be in agricultural use (Grade 2 and small part Grade 3).
	1.2 Reduce the use of non-renewable resources including energy sources	0 There is no evidence to suggest whether this site is particularly suitable or unsuitable for sustainable design and construction and renewable energy technologies. Effects are expected to be dependent on the development of the masterplan.
	1.3 Limit water consumption to levels supportable by natural processes and storage systems	0? The absolute effect of further development will be to put additional strain on already stretched water resources in the sub-region, therefore possibly leading to lowered groundwater levels and impacts on natural processes.  In relative terms, there is little evidence to suggest that development at this location in South Cambs would lead to a risk of particular effects on water resources, either by water abstraction or water pollution (this is not a source protection zone).  However, the Environment Agency has expressed concern with regard to the impacts on the recipient watercourse from the Sewage Treatment Works (STW) which would be the likely recipient of foul water drainage from development in this area.  Any development at this location would be required to demonstrate that there was enough capacity within the recipient foul water drainage system, or that the system could be upgraded in order to facilitate any proposed growth.

Biodiversity	2.1 Avoid damage to designated sites and protected species	<p>-?</p> <p>Caldecote Meadows SSSI is around 300m from the proposed site, and around 900m to south east of the area indicated for development in the objector's masterplan. This is an important consideration, and until it is established what potential there is to impact directly or indirectly on the SSSI this must be considered a significant negative effect. It will be important to consider the full range of potential indirect effects that could impact on the ecological health of the SSSI, including recreation, mis-use, pollution and impacts on non-designated habitats in the vicinity of the SSSI that might in some ways support its ecological functioning.</p> <p>Furthermore, Bucket Hill Plantation County Wildlife Site lies in the south east corner of the site, although it appears to be outside the area proposed for built development.</p> <p>The site also supports legally protected species including great crested newt, bats, and a number of bird species of conservation concern (red and amber list species), and national and local BAP species including brown hare, skylark and song thrush.</p>
	2.2 Maintain and enhance the range and viability of characteristic habitats and species	<p>-?</p> <p>This is a relatively diverse site containing a number of additional non designated habitats of varying ecological interest and significance including broadleaved woodland, veteran trees, ponds and species rich hedgerows.</p> <p>The masterplan and ecological survey indicate the potential for habitat enhancement in association with development. However, the term enhancement must be used with caution. The key aim must be to ensure that existing species and habitats of conservation concern are not lost or impacted upon unduly. It will be important to consider that the value of existing habitat features may come partly as a result of them existing as part of a matrix of features that in some ways interact and function together and as part of the wider landscape. The aim should not be simply to 'increase biodiversity' at the site level.</p>
	2.3 Improve opportunities for people to access and appreciate wildlife and wild places	<p>+?</p> <p>This site has good access to the countryside via public footpath.</p>

Landscape, townscape and archaeology	3.1 Avoid damage to areas and sites designated for their historic interest, and protect their settings	-? The site is located in a landscape of high archaeological potential. The County Council would need further information before they could provide detailed advice on the suitability of the site for development, but on the basis of current information would object to the allocation of this site, as development could have a severe detrimental impact on the archaeological landscape.  There are some listed buildings in the vicinity of the site, but it is likely that development proposals can be sympathetic to their settings so that there will be no significant adverse impact.
	3.2 Maintain and enhance the diversity and distinctiveness of landscape and townscape character	- Development at this location would result in a linear form of development along the A428 corridor that would be perceived as continuous development some 7 kilometres in length extending into the countryside from the outer boundary of the Cambridge Green Belt.
	3.3 Create places, spaces and buildings that work well, wear well and look good	0 Effects will depend on the final masterplan
Climate change and pollution	4.1 Reduce emissions of greenhouse gasses and other pollutants (including air, water, soil, noise, vibration and light)	- This site is not particularly well located in terms of minimising car dependency. Although this site is slightly closer to Cambridge than Cambourne, it is the expectation that the modal share of private car use will remain high.  The centre of this site is over 2km from the centre of Cambourne. If the vehicular arrangements were such that it was more difficult to drive to Cambourne than walk/cycle/go by bus then it would be reasonable to expect a proportion of residents to use the sustainable modes of transport for local trips to the centre of Cambourne from this site, but the distance may limit this.
	4.2 Minimise waste production and support the recycling of waste products	0 At this stage there is no evidence to suggest how suitable development at this location will be for adopting sustainable waste management practices.
	4.3 Limit or reduce vulnerability to the effects of climate change (including flooding)	? The site is not expected to be at risk of flooding. However, there is concern about contributing to / increasing flood risk in surrounding areas, which would need to be addressed in any Flood Risk Assessment.

Healthy communities	5.1 Maintain and enhance human health	<p>?-</p> <p>The north of the site is bounded by the A428, which is a source of both air pollution and noise pollution. Both of these forms of pollution are likely to increase from their baseline levels as a result of future development at this site and elsewhere in the sub-region. However, given adequate mitigation (particularly buffering between the road and housing), it is unlikely that there will be any significant effects on human health.</p> <p>Traffic noise and noise from any commercial / industrial / agricultural uses that will remain on the periphery require careful consideration. There are existing concerns about noise emissions from these industrial units and it is recommended that this noise constraint is fully considered prior to any allocation.</p> <p>There is the potential for land contamination and so this would require investigation and potentially remediation.</p>
	5.2 Reduce and prevent crime, and reduce the fear of crime	<p>0</p> <p>There is no evidence available to suggest whether this location will be particularly suited or unsuited to preventing crime or the fear of crime. Much will depend on the final masterplan.</p>
	5.3 Improve the quantity and quality of publicly accessible open space	<p>+?</p> <p>The site is larger than the proposed development and therefore there is potential for the incorporation of open space.</p> <p>There is good access to open countryside via public footpaths.</p>

Inclusive communities	6.1 Improve the quality, range and accessibility of services and facilities (e.g. health, transport, education, training, leisure opportunities)	<p>?- This site is separate to Cambourne, with existing services and facilities in Cambourne a considerable walk away from the site. It is likely that the development would function as a separate village with its own services and facilities. The objector proposes that the development would have its own high street, shopping, leisure and community facilities and also a broader range of employment opportunities.</p> <p>The physical scope to enhance or provide new facilities within the existing village centre is limited and therefore there is limited potential to provide higher order services and facilities that would help make Cambourne more inherently sustainable.</p> <p>However, new facilities provided in the new development would not be very accessible from the existing village. It could therefore be considered that a new centre would compromise the overall relative sustainability of Cambourne in terms of provision of adequate services and facilities in a central accessible location.</p> <p>Furthermore, new facilities in the objection site would need to be of a level that would not compromise the viability of the existing centre that is now getting established.</p> <p>The nearest secondary school is about 9 km. The County Council has advised that there is a need for a secondary school to serve Cambourne based on current planned growth which is being explored, and that provision is not dependent on additional growth. However, additional development would add to the demand for secondary places and would also offer the opportunity for identifying a site for a secondary school.</p>
	6.2 Redress inequalities related to age, gender, disability, race, faith, location and income	<p>0 It is not thought that there are particular local deprivational inequalities that could be addressed in anyway by development at this location.</p>
	6.3 Ensure all groups have access to decent, appropriate and affordable housing	<p>+ Development at Cambourne would contribute towards meeting the housing needs that exist in South Cambridgeshire District. However, Cambourne is some distance away from Cambridge, where the greatest housing needs can be found.</p>
	6.4 Encourage and enable the active involvement of local people in community activities	<p>? Much will depend on the final masterplan. However, it is important to consider the impact that further expansion of Cambourne will have the sense of place and sense of community felt by those existing residents of Cambourne.</p>

Economic activity	7.1 Help people gain access to satisfying work appropriate to their skills, potential and place of residence	? There is a considerable amount of employment development in Cambourne, which will be easily accessible from the site. However, this site is not well located in terms of the major employment locations at the Cambridge Science Park (12.5km), Addenbokes (15km) and Cambridge City Centre (11km / 45 minute bus journey away).  The objector is proposing new employment and a new centre with community facilities as part of the development.
	7.2 Support appropriate investment in people, places, communications and other infrastructure	0 There will be no significant effects in terms of this SA Objective.
	7.3 Improve the efficiency, competitiveness, vitality and adaptability of the local economy	0 There will be no significant effects in terms of this SA Objective

**Summary**

This is a mixed greenfield and brownfield development on a site that includes some ecological interest on-site as well as being in close proximity to important sites for biodiversity and is also important in terms of visual landscape value. Furthermore, the archaeological potential of the site is a constraint to be addressed, but is unlikely to be an absolute constraint.

Cambourne is the least sustainable of the Rural Centres in terms of location relative to Cambridge and quality of public transport. Reliance on the private car to access services and facilities as well as employment would most likely be relatively high, despite the fact that the proposals to develop a new local centre.

Development on Bourn Airfield would not relate well to the existing form of Cambourne and would not provide a sustainable extension to the village in the sense that it is questionable whether walking or cycling will be an attractive form of movement between the two centres. There are a number of questions regarding whether provision of a new centre will compliment the services and facilities available at Cambourne. The location of this development does, however, mean that there is good access to open and biodiversity rich countryside as well as other open space.

**Recommendations**

If this location is to go forward for further consideration it will be important to:

1. Consider foul water drainage constraints.
2. Consider the full range of potential effects that could impact on the ecological health of the SSSI and incorporate existing biodiversity features into the masterplan in such a way that allows them to retain their functional value.
3. Clarify the nature of the impact to the County Wildlife Site
4. Give consideration to the high archaeological potential of the site.
5. Establish the off-site impacts of increased surface water run-off at this location.
6. Consider how residents might access services, facilities and employment easily by



means other than the private car.

7. Consider effects on the sustainability of the existing village centre at Cambourne
8. Consider how effects on the community / sense of place in Cambourne might be mitigated.

### Assessment table for Bourne Airfield (2), Cambourne

SA Topic	SA Objective	Assessment findings
Land and water resources	1.1 Minimise the irreversible loss of undeveloped land and productive agricultural holdings	? The site comprises a mix of uses and features, including runways, buildings and open land, much of which appears to be in agricultural use (Grade 2).
	1.2 Reduce the use of non-renewable resources including energy sources	0 There is no evidence to suggest whether this site is particularly suitable or unsuitable for sustainable design and construction and renewable energy technologies. Effects are expected to be dependent on the development of the masterplan.
	1.3 Limit water consumption to levels supportable by natural processes and storage systems	0? The absolute effect of further development will be to put additional strain on already stretched water resources in the sub-region, therefore possibly leading to lowered groundwater levels and impacts on natural processes.  In relative terms, there is little evidence to suggest that development at this location in South Cambs would lead to a risk of particular effects on water resources, either by water abstraction or water pollution (this is not a source protection zone).  However, the Environment Agency has expressed concern with regard to the impacts on the recipient watercourse from the Sewage Treatment Works (STW) which would be the likely recipient of foul water drainage from development in this area.  Any development at this location would be required to demonstrate that there was enough capacity within the recipient foul water drainage system, or that the system could be upgraded in order to facilitate any proposed growth.

Biodiversity	2.1 Avoid damage to designated sites and protected species	<p>-?</p> <p>Caldecote Meadows SSSI is around 1000m from the objection site, and around 1300m to south east of the area indicated for development in the objector's masterplan. This is an important consideration, and until it is established what potential there is to impact directly or indirectly on the SSSI this must be considered a significant negative effect. It will be important to consider the full range of potential indirect effects that could impact on the ecological health of the SSSI, including recreation, mis-use, pollution and impacts on non-designated habitats in the vicinity of the SSSI that might in some ways support its ecological functioning.</p> <p>The site also supports legally protected species including great crested newt, bats, and a number of bird species of conservation concern (red and amber list species), and national and local BAP species including brown hare, skylark and song thrush.</p>
	2.2 Maintain and enhance the range and viability of characteristic habitats and species	<p>-?</p> <p>This is a relatively diverse site containing a number of additional non designated habitats of varying ecological interest and significance including broadleaved woodland, veteran trees, ponds and species rich hedgerows.</p> <p>The masterplan and ecological survey indicate the potential for habitat enhancement in association with development. However, the term enhancement must be used with caution. The key aim must be to ensure that existing species and habitats of conservation concern are not lost or impacted upon unduly. It will be important to consider that the value of existing habitat features may come partly as a result of them existing as part of a matrix of features that in some ways interact and function together and as part of the wider landscape. The aim should not be simply to 'increase biodiversity' at the site level.</p>
	2.3 Improve opportunities for people to access and appreciate wildlife and wild places	<p>+?</p> <p>This site has good access to the countryside via public footpath.</p>

Landscape, townscape and archaeology	3.1 Avoid damage to areas and sites designated for their historic interest, and protect their settings	-? The site is located in a landscape of high archaeological potential. The County Council would need further information before they could provide detailed advice on the suitability of the site for development, but on the basis of current information would object to the allocation of this site, as development could have a severe detrimental impact on the archaeological landscape.  There are some listed buildings in the vicinity of the site, but it is likely that development proposals can be sympathetic to their settings so that there will be no significant adverse impact.
	3.2 Maintain and enhance the diversity and distinctiveness of landscape and townscape character	- Same issues arise as with the larger Bourn Airfield (1), however in this case are not as significant.  Development at this location would result in a linear form of development along the A428 corridor that would be perceived as continuous development some 7 kilometres in length extending into the countryside from the outer boundary of the Cambridge Green Belt.
	3.3 Create places, spaces and buildings that work well, wear well and look good	0 Effects will depend on the final masterplan
Climate change and pollution	4.1 Reduce emissions of greenhouse gasses and other pollutants (including air, water, soil, noise, vibration and light)	- This site is not particularly well located in terms of minimising car dependency. Although this site is slightly closer to Cambridge than Cambourne, it is the expectation that the modal share of private car will remain high.  The centre of this site is over 2km from the centre of Cambourne. If the vehicular arrangements were such that it was more difficult to drive to Cambourne than walk/cycle/go by bus then it would be reasonable to expect a proportion of residents to use the sustainable modes of transport for local trips to the centre of Cambourne from this site, but the distance may limit this.
	4.2 Minimise waste production and support the recycling of waste products	0 At this stage there is no evidence to suggest how suitable development at this location will be for adopting sustainable waste management practices.
	4.3 Limit or reduce vulnerability to the effects of climate change (including flooding)	? The site is not expected to be at risk of flooding. However, there is concern about contributing to / increasing flood risk in surrounding areas, which would need to be addressed in any Flood Risk Assessment.

Healthy communities	5.1 Maintain and enhance human health	<p>?-</p> <p>The north of the site is bounded by the A428, which is a source of both air pollution and noise pollution. Both of these forms of pollution are likely to increase from their baseline levels as a result of future development at this site and elsewhere in the sub-region. However, given adequate mitigation (particularly buffering between the road and housing), it is unlikely that there will be any significant effects on human health.</p> <p>Traffic noise and noise from any commercial / industrial / agricultural uses that will remain on the periphery require careful consideration. There are existing concerns about noise emissions from these industrial units and it is recommended that this noise constraint is fully considered prior to any allocation.</p> <p>There is the potential for land contamination and so this would require investigation and potentially remediation.</p>
	5.2 Reduce and prevent crime, and reduce the fear of crime	<p>0</p> <p>There is no evidence available to suggest whether this location will be particularly suited or unsuited to preventing crime or the fear of crime. Much will depend on the final masterplan.</p>
	5.3 Improve the quantity and quality of publicly accessible open space	<p>+?</p> <p>The site is larger than the proposed development and therefore there is potential for the incorporation of open space.</p> <p>There is good access to open countryside via public footpaths.</p>
Inclusive communities	6.1 Improve the quality, range and accessibility of services and facilities (e.g. health, transport, education, training, leisure opportunities)	<p>-?</p> <p>This is a smaller level of development compared to Bourn Afield 1 and is not clear whether it would be sufficient to support the high street, shopping, leisure and community facilities as proposed by the objector for the larger scale development.</p> <p>This site is separate to Cambourne, with existing services and facilities in Cambourne a considerable walk away from the site.</p> <p>The nearest secondary school is about 9 km. The County Council has advised that there is a need for a secondary school to serve Cambourne based on current planned growth which is being explored, and that provision is not dependent on additional growth. However, additional development would add to the demand for secondary places and would also offer the opportunity for identifying a site for a secondary school.</p> <p>Note: surprising impact is neutral given access constraints.</p>
	6.2 Redress inequalities related to age, gender, disability, race, faith, location and income	<p>0</p> <p>It is not thought that there are particular local derivational inequalities that could be addressed in anyway by development at this location.</p>

	6.3 Ensure all groups have access to decent, appropriate and affordable housing	+ Development at Cambourne would contribute towards meeting the housing needs that exist in South Cambridgeshire District. However, Cambourne is some distance away from Cambridge, where the greatest housing needs can be found.
	6.4 Encourage and enable the active involvement of local people in community activities	? Much will depend on the final masterplan. However, it is important to consider the impact that further expansion of Cambourne will have the sense of place and sense of community felt by those existing residents of Cambourne.
Economic activity	7.1 Help people gain access to satisfying work appropriate to their skills, potential and place of residence	? There is a considerable amount of employment development in Cambourne, which will be easily accessible from the site. However, this site is not well located in terms of the major employment locations at the Cambridge Science Park (12.5km), Addenbokes (15km) and Cambridge City Centre (11km / 45 minute bus journey away).
	7.2 Support appropriate investment in people, places, communications and other infrastructure	0 There will be no significant effects in terms of this SA Objective.
	7.3 Improve the efficiency, competitiveness, vitality and adaptability of the local economy	0 There will be no significant effects in terms of this SA Objective
<p><b>Summary</b></p> <p>This is a mixed greenfield and brownfield development on a site that includes some ecological interest on-site as well as being in close proximity to important sites for biodiversity and is also important in terms of visual landscape value. Furthermore, the archaeological potential of the site is a constraint to be addressed, but is unlikely to be an absolute constraint. The fact that this proposal has a reduced development footprint may mean that some effects are less significant relative to the Bourn Airfield (1) proposal.</p> <p>Cambourne is the least sustainable of the Rural Centres in terms of location relative to Cambridge and quality of public transport. Reliance on the private car to access services and facilities as well as employment would most likely be relatively high, despite the fact that the proposals to develop a new local centre.</p> <p>Development on Bourn Airfield would not relate well to the existing form of Cambourne and would not provide a sustainable extension to the village in the sense that it is questionable whether walking or cycling will be an attractive form of movement between the two centres. It is currently uncertain whether the development would be of a scale to deliver its own centre. The location of this development does, however, mean that there is good access to open and biodiversity rich countryside as well as other open space and play areas.</p> <p><b>Recommendations</b></p> <p>If this location is to go forward for further consideration it will be important to:</p> <ol style="list-style-type: none"> <li>1. Consider foul water drainage constraints.</li> </ol>		

2. Consider the full range of potential effects that could impact on the ecological health of the SSSI and incorporate existing biodiversity features into the masterplan in such a way that allows them to retain their functional value.
3. Clarify the nature of the impact to the County Wildlife Site
4. Give consideration to the high archaeological potential of the site.
5. Establish the off-site impacts of increased surface water run-off at this location.
6. Consider how residents might access services, facilities and employment easily by means other than the private car.
7. Consider how effects on the community / sense of place in Cambourne might be mitigated.

## Appendix III: Preferred options assessment matrices



**Preferred Option Policy: Land between Huntingdon Road, Histon Road and the A14, North West Cambridge (South Cambs revision)**

SA Topic	SA objectives	Decision-making criteria	Short term	Med term	Long term	Comments
1. Land and water resources	1.1 Minimise the irreversible loss of undeveloped land and productive agricultural holdings	Will it use land that has been previously developed?	--	--	--	Development at this site will result in loss of Green Belt, but the Policy seeks to mitigate effects, including by stating that: <i>"The Council considers this area to be important to the Green Belt setting of Cambridge and if land is to be allocated for development, only that land should be released from the Green Belt with the remainder of the triangle remaining Green Belt."</i> Nonetheless the net residual effect in terms of this objective will be negative.
		Will it use land efficiently?	+	+	+	
		Will it protect and enhance the best and most versatile agricultural land?	-	-	-	
	1.2 Reduce the use of non-renewable resources, including energy sources	Will it reduce emissions of greenhouse gases by reducing energy consumption?	?	?	?	It appears that the Policy may require homes approved on or before 31 March 2013 to meet Code for Sustainable Homes Level 4, and all homes approved after this date to meet Code Level 5.

		<p>Will it lead to an increased proportion of energy and other resources being met from renewable sources?</p>	?	?	?	<p><b>Recommendation:</b> Clarify the wording relating to Code for Sustainable Homes requirements.</p> <p>The Policy also states that: <i>“A proportion of the development will use cutting-edge technologies that fully address sustainability issues and minimise any environmental impact by pushing at the boundaries of the proven technology available at the time each exemplar project comes forward. The exemplar requirement could be met, in part, by providing an increased level of sustainability across the development as a whole materially above current requirements.”</i></p> <p><b>Recommendation:</b> Add clarity to this requirement for exemplar developments.</p> <p>It is noted that as this is a residential led development there is no consideration of requiring non-residential developments to meet certain BREEAM sustainability standards.</p>
	1.3 Limit water consumption to	Will it reduce water consumption?	-	-	-	The absolute effect of further development will be to put additional strain on already

	levels supportable by natural processes and storage systems	Will it conserve ground water resources?	-	-	-	<p>stretched water resources in the sub-region, therefore possibly leading to lowered groundwater levels and impacts on natural processes.</p> <p>However, the Policy has a major emphasis on water conservation, stating that:</p> <p><i>“All development at North West Cambridge will incorporate water conservation measures including water saving devices, rainwater harvesting and greywater recycling measures.”</i></p> <p>The Policy also includes specific targets for levels of water efficiency that must be achieved.</p> <p>Therefore, water efficiency per capita may increase but overall water consumption in the Cambridge Sub-region will not be reduced.</p>
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2. Biodiversity	2.1 Avoid damage to designated sites and protected species	Will it protect sites designated for nature conservation interest?	+	+	++	<p>The Policy requires developers to propose a well evidenced biodiversity strategy as well as a Biodiversity Management Strategy to ensure high quality implementation, adoption and maintenance.</p> <p>It is noted that there is a focus on 'biodiversity areas' rather than on fully integrating biodiversity as part of the development.</p> <p>There is no specific reference to off-site impacts on biodiversity, but the commitment to undertaking a Countryside Enhancement Strategy appears to be a proactive approach to ensuring the ecological health of the wider landscape.</p> <p>The net effect should be that significant negative impacts to designated sites and protected species should be avoided.</p>
	2.2 Maintain and enhance the range and viability of characteristic habitats and species	Will it conserve species, reversing declines, and help to enhance diversity?	+	+	++	<p>See assessment against SA Objective 2.1 above.</p> <p>Furthermore, there is a focus on maximising the multi-functional value of drainage systems, including the biodiversity value, and protecting the water environment.</p> <p>The net effect should be that significant negative impacts to biodiversity should be avoided.</p>
		Will it reduce habitat fragmentation?	+	+	++	
		Will it help achieve Biodiversity Action Plan targets?	+	+	++	

	<p>2.3 Improve opportunities for people to access and appreciate wildlife and wild places</p>	<p>Will it improve access to wildlife, and wild places?</p>	+	+	+	<p>A Countryside Enhancement Strategy will be prepared and implemented to provide landscape, biodiversity and public access enhancements in the surrounding countryside.</p>
		<p>Will it maintain and, where possible, increase the area of high-quality green space in the District?</p>	+	+	+	
		<p>Will it promote understanding and appreciation of wildlife?</p>	+	+	+	
		<p>Will it improve access to the wider countryside through the network of public rights of way?</p>	+	+	+	
<p>3. Landscape, townscape and archaeology</p>	<p>3.1 Avoid damage to areas and sites designated for their historic interest, and protect their settings.</p>	<p>Will it protect or enhance sites, features of areas of historical, archaeological, or cultural interest (including conservation areas, listed buildings, registered parks and gardens and scheduled monuments)?</p>	?	?	?	<p>There is a focus on protecting the setting of Cambridge, but no other mention of protecting archaeological assets or the historic environment more generally. It is likely that protection of any local archaeological value will be achieved through Development Control policy.</p>

	3.2 Maintain and enhance the diversity and distinctiveness of landscape and townscape character	Will it maintain and enhance the diversity and distinctiveness of landscape and townscape character?	--	-	-	<p>Development at this site will have a significant negative impact on the Cambridge Green Belt, particularly in the short-term. There is a major focus within the Policy on mitigating landscape impacts, stating that:</p> <p><i>“A Landscape Strategy for North West Cambridge must be submitted and approved prior to the granting of any planning permission, of a level of detail appropriate to the type of application. It will establish the principles for structural landscaping within and on the countryside edges of the urban extension and ensure the creation of a quality environment which makes best use of and enhances existing tree and hedge resources as a setting for the development and ensures that any alterations to topography within the urban extension are appropriate to local landscape character. Key views of Cambridge will be respected.”</i></p> <p>Nonetheless the net residual effect in terms of this objective will be negative.</p>
		Will it protect and enhance open spaces of amenity and recreational value?	++	++	++	
		Will it maintain and enhance the character of settlements?	--	-	-	
	3.3 Create places, spaces and buildings that work well, wear well and look good	Will it improve the satisfaction of people with their neighbourhoods as places to live?	-	+	+	<p>There is a requirement for Masterplans to be submitted alongside design guidance and design codes for each phase of the proposed development. The Policy also states that:</p> <p><i>“The development will create a balanced and</i></p>

		Will it lead to developments built to a high standard of design, and good place making?	+	+	+	<p><i>socially inclusive community, with a strong sense of local identity, where people can live in a healthy and safe environment. It will be well designed to a high quality, have a permeable, legible and flexible design that is well integrated with development proposed in Cambridge City, and have appropriate edge treatments that respect the Green Belt setting of Cambridge.</i></p> <p>Effects may be negative in the short-term, during the construction period.</p>
4. Climate change and pollution	4.1 Reduce emissions of greenhouse gasses and other pollutants (including air, water, soil, noise, vibration and light)	Will it reduce emissions of greenhouse gases?	+	+	+	<p>The Policy includes proposals to enhance the attractiveness of sustainable modes of transport, including by stating that:</p> <p><i>“High Quality Public Transport will be provided to serve the development, including segregated bus priority through the development linking effectively with the route through the adjoining development in Cambridge City and into the wider bus network. Travel plans will include measures to encourage public transport use and all development will be within 400m easy walking distance of a High Quality Public Transport bus stop. There will be a network of dedicated, highly accessible, segregated, high quality, safe, direct, connected and convenient rights of way, including cycle and pedestrian routes, both within the development and connecting with the rest of Cambridge, surrounding villages, the open countryside and the wider rights of way</i></p>
		Will it improve air quality?	?	?	?	
		Will it reduce traffic volumes?	?	?	?	
		Will it support travel by means other than the car?	+	+	+	
		Will it reduce levels of noise or noise concerns?	?	?	?	
		Will it reduce or minimise light pollution?	0	0	0	

		Will it improve water quality including by reducing diffuse and point source water pollution?	+	+	+	<p><i>network.”</i></p> <p>As a result of this Policy, it is felt that there is the potential for this development to allow people to live more sustainably, with a smaller carbon footprint.</p> <p>There is the potential for development at this location to result in increased traffic congestion on the A14. The Policy has an emphasis on ensuring that development is phased so as to minimise traffic congestion impacts on the A14, including during the construction phase. Absolute effects are therefore uncertain.</p> <p>There is also the potential for negative effects in terms of air and noise pollution. Considerable attention is given to minimising and mitigating noise and air quality impacts associated with the A14. The Policy also requires that consideration is given to the potential for traffic growth to worsen pollution associated with the A14. Absolute effects are therefore uncertain.</p>
4.2 Minimise waste production and support the recycling of waste products		Will it reduce household waste?	+	+	+	The Policy seeks to promote good waste management, including at the construction phase.
		Will it increase waste recovery and recycling?	+	+	+	



	4.3 Limit or reduce vulnerability to the effects of climate change (including flooding)	Will it minimise risk to people and property from flooding, storm events or subsidence?	+	+	+	The Policy states that: <i>“The surface water drainage system will only release surface water run-off into the water courses surrounding North West Cambridge at an equal or slower rate than is the case prior to development... A Strategic Surface Water Drainage Strategy will be required which must consider the impact of the development on the wider catchment, including having regard to other proposed development. All flood mitigation measures should make allowance for the forecast effects of climate change. If practicable, such measures will take the opportunity to mitigate existing flood risk to Histon and Impington villages.”</i>
5. Healthy communities	5.1 Maintain and enhance human health	Will it reduce substantially reduce mortality rates?	0	0	0	The Policy should result in excellent opportunities for cycling as well as access to open space and the countryside.
		Will it encourage healthy lifestyles, including travel choices?	+	+	+	
	5.2 Reduce and prevent crime, and reduce the fear of crime	Will it reduce actual levels of crime?	?	?	?	Despite the scale of the development, the Policy does not make reference to designing out crime. However, designing out crime is addressed by Development Control policy.
		Will it reduce fear of crime?	?	?	?	

	5.3 Improve the quantity and quality of publicly accessible open space	Will it increase the quantity and quality of publicly accessible open space?	+	+	+	<p>The following integrated approach to open space delivery is promoted, which should ensure high quality open space:</p> <p><i>“there should be a single agreed management strategy covering recreation, landscape and biodiversity. The inclusion of water and drainage features within open spaces would have significant advantages and should therefore be investigated.”</i></p>
6. Inclusive communities	6.1 Improve the quality, range and accessibility of services and facilities (e.g. health, transport, education, training, leisure opportunities)	Will it improve the quality and range of services and facilities, including health, education, shopping, sport, leisure, arts and cultural activities?	?	+	+	<p>The Policy refers to the importance of providing adequate local services and facilities as well as allowing for good access by public transport, walking and cycling.</p> <p>It is stated that planning obligations may be used to deliver services and facilities in the adjoining development in Cambridge City.</p>
		Will it improve accessibility to key local services and facilities, including health, education and leisure (shops, post offices, pubs etc)?	?	+	+	
		Will it improve accessibility by means other than the car and improve the attractiveness of environmentally better modes including public transport, cycling and walking?	?	+	+	

		Will it support and improve community and public transport?	+	+	+	
6.2 Redress inequalities related to age, gender, disability, race, faith, location and income	Will it improve relations between people from different backgrounds or social groups?	0	0	0	<p>It is stated that all homes will be within 400m of a bus stop.</p> <p>It is noted that planning obligations may be used to deliver services and facilities in the adjoining development in Cambridge City.</p> <p>It would appear that the majority if not all local services and facilities will be delivered as part of the local centre that will form part of the adjacent city development.</p> <p><b>Recommendation:</b> Demonstrate that services and facilities will be adequately accessible for all members of society, including the elderly and disabled, from the day that people move into the new development.</p>	
	Will it reduce poverty and social exclusion in those areas most affected?	0	0	0		
	Will it promote accessibility for all members of society, including the elderly and disabled?	?	?	?		
6.3 Ensure all groups have access to decent, appropriate and affordable housing	Will it support the provision of a range of housing types and sizes, including affordable and key worker housing, to meet the identified needs of all sectors of the community?	+?	+?	+?	<p>The Policy states that:</p> <p><i>“Approximately 920 dwellings will be provided in South Cambridgeshire... with a good mix of house types, sizes and tenures (including affordable housing) attractive to, and meeting the needs of, all ages and sectors of society including those with disabilities.</i></p> <p><i>The starting point for negotiations concerning</i></p>	

		Will it reduce the number of unfit homes?	0	0	0	<p><i>the provision of affordable housing at North West Cambridge will be Policy HG/3 of the Development Control Policies DPD. However, this is a major development, and a balance may need to be struck between competing requirements, in the light of economic viability."</i></p> <p><b>Recommendation:</b> Clarify why a 'balance may need to be struck' in terms of the provision of affordable housing.</p> <p>The Policy states that provision will be made for around 8 pitches for Gypsy and Traveller accommodation. Reference is made to Government guidance - 'Designing Gypsy and Traveller Sites - Good Practice Guide' (DCLG May 2008) – which provides examples of how small scale sites can be successfully located in urban areas.</p>
		Will it meet the needs of the travelling community?	+	+	+	
		6.4 Encourage and enable the active involvement of local people in community activities	Will it increase the ability of people to influence decisions?	0	0	
		Will it encourage engagement with community activities?	0	0	0	No significant effects
7.Economic activity	7.1 Help people gain access to satisfying work appropriate to	Will it encourage businesses development?	0	0	0	This is a residential-led development that will provide homes for people in a location where they can access local employment areas by

	their skills, potential and place of residence	Will it improve the range of employment opportunities to provide a satisfying job or occupation for everyone who wants one?	0	0	0	means other than the car.
		Will it improve accessibility to local employment by means other than the car?	+	+	+	
		Will it encourage the rural economy and diversification?	0	0	0	
7.2 Support appropriate investment in people, places, communications and other infrastructure		Will it improve the level of investment in key community services and infrastructure?	+	+	+	The Policy gives some guidance as to how monies collected through planning obligations should be spent in order to best deliver essential community infrastructure.
		Will it support provision of key communications infrastructure, including broadband?	0	0	0	
		Will it improve access to education and training, and support provision of skilled employees to the economy?	0	0	0	

7.3 Improve the efficiency, competitiveness, vitality and adaptability of the local economy	Will it improve business development and enhance competitiveness?	0	0	0	This is a residential-led development that will provide workers to support the growth of the Cambridge economy.
	Will it support the Cambridge area's position as a world leader in research and technology based industries, higher education and research, particularly through the development and expansion of clusters?	+	+	+	
	Will it support sustainable tourism?	0	0	0	
	Will it protect the shopping hierarchy, supporting the vitality and viability of Cambridge City Centre, town, district, and local centres?	0	0	0	

**Summary:**

This is a detailed policy which focuses on ensuring that a number of the key sustainability issues presented by major development at this location are addressed. In particular, the Policy focuses on mitigating landscape impacts associated with the loss of an important area of Green Belt and ensuring water efficiency and minimising flood-risk. Traffic congestion and air / noise pollution associated with the A14 is also a constraint that is given careful consideration by the Policy. The Policy also seeks to make the most of locational opportunities by promoting an integrated approach to delivering biodiversity and high quality landscapes alongside the provision of open space and countryside access. This is a location that has the potential to allow for sustainable transport patterns amongst residents. The Policy seeks to build on this potential by stating what will be delivered on the ground in terms of public transport and high quality cycling and walking routes.

Nonetheless, there will be some residual negative sustainability impacts that cannot be wholly mitigated by the Policy. Foremost is the issue regarding the loss of productive agricultural land in the Green Belt and the impacts of this on the landscape setting of Cambridge. Effects may be particularly severe during the construction phase, reducing over time as a result of landscaping. Similarly, development at this location will put strain on water resources and potentially lead to negative effects in terms of air quality and noise objectives, but effects will be mitigated to some degree by the requirements of the Policy.

**Recommendations:**

1. Clarify why a 'balance may need to be struck' in terms of the provision of affordable housing.
2. Demonstrate that services and facilities will be adequately accessible for all members of society, including the elderly and disabled, from the day that people move into the new development.
3. Add clarity to this requirement for exemplar developments.

### Preferred Option Policy: Powell's Garage, Great Shelford

SA Topic	SA objectives	Decision-making criteria	Short term	Med term	Long term	Comments
1. Land and water resources	1.1 Minimise the irreversible loss of undeveloped land and productive agricultural holdings	Will it use land that has been previously developed?	+	+	+	A relatively high density of development is promoted on this brownfield site.
		Will it use land efficiently?	+	+	+	
		Will it protect and enhance the best and most versatile agricultural land?	+	+	+	
	1.2 Reduce the use of non-renewable resources, including energy sources	Will it reduce emissions of greenhouse gases by reducing energy consumption?	?	?	?	It is stated that this development does not lend itself to being an exemplar development due to its small size, and so standards of sustainable design and construction will be governed by Development Control Policy. It may be that energy use in the home per capita is reduced as a result, but there remains some uncertainty.
		Will it lead to an increased proportion of energy and other resources being met from renewable sources?	?	?	?	
	1.3 Limit water consumption to	Will it reduce water consumption?	-	-	-	The absolute effect of further development will be to put additional strain on already



	levels supportable by natural processes and storage systems	Will it conserve ground water resources?	-	-	-	stretched water resources in the sub-region, therefore possibly leading to lowered groundwater levels and impacts on natural processes. It is stated that this development does not lend itself to being an exemplar development due to its small size, and so standards of sustainable design and construction will be governed by Development Control Policy. Water efficiency per capita may increase but overall water consumption in the Cambridge Sub-region will not decrease.
2. Biodiversity	2.1 Avoid damage to designated sites and protected species	Will it protect sites designated for nature conservation interest?	0	0	0	No potential for significant effects
	2.2 Maintain and enhance the range and viability of characteristic habitats and species	Will it conserve species, reversing declines, and help to enhance diversity?	0	0	0	No potential for significant effects
		Will it reduce habitat fragmentation?	0	0	0	
		Will it help achieve Biodiversity Action Plan targets?	0	0	0	
	2.3 Improve opportunities for people to access and	Will it improve access to wildlife, and wild places?	0	0	0	No potential for significant effects

	appreciate wildlife and wild places	Will it maintain and, where possible, increase the area of high-quality green space in the District?	0	0	0	
		Will it promote understanding and appreciation of wildlife?	0	0	0	
		Will it improve access to the wider countryside through the network of public rights of way?	0	0	0	
3. Landscape, townscape and archaeology	3.1 Avoid damage to areas and sites designated for their historic interest, and protect their settings.	Will it protect or enhance sites, features of areas of historical, archaeological, or cultural interest (including conservation areas, listed buildings, registered parks and gardens and scheduled monuments)?	-	?	?	Whether or not there are significant effects on the designated conservation area will depend on the sensitivity of building design. The Policy states that: <i>“Development must preserve the character and appearance of the Conservation Area and provide a high quality landmark design in this sensitive location.”</i> However, there will certainly be potential for negative impacts during the construction phase of development.
	3.2 Maintain and enhance the diversity and distinctiveness of landscape and townscape character	Will it maintain and enhance the diversity and distinctiveness of landscape and townscape character?	-	?	?	There may be the potential for this development to add to the character of the settlement, although during the construction phase negative impacts would be likely.

		Will it protect and enhance open spaces of amenity and recreational value?	0	0	0	
		Will it maintain and enhance the character of settlements?	-	?	?	
	3.3 Create places, spaces and buildings that work well, wear well and look good	Will it improve the satisfaction of people with their neighbourhoods as places to live?	0	0	0	The Policy states that: <i>“Development must preserve the character and appearance of the Conservation Area and provide a high quality landmark design in this sensitive location.”</i>
		Will it lead to developments built to a high standard of design, and good place making?	+	+	+	
4. Climate change and pollution	4.1 Reduce emissions of greenhouse gasses and other pollutants (including air, water, soil, noise, vibration and light)	Will it reduce emissions of greenhouse gases?	+	+	+	The supporting text highlights that this location is well served by public transport and also local services and facilities, and as a result it is a suitable location for a relatively high density development. As a result of this Policy, it is felt that there is the potential for this development to allow people to live more sustainably, with a smaller carbon footprint.
		Will it improve air quality?	0	0	0	
		Will it reduce traffic volumes?	0	0	0	
		Will it support travel by means other than the car?	+	+	+	

		Will it reduce levels of noise or noise concerns?	0	0	0		
		Will it reduce or minimise light pollution?	0	0	0		
		Will it improve water quality including by reducing diffuse and point source water pollution?	0	0	0		
	4.2 Minimise waste production and support the recycling of waste products	Will it reduce household waste?	0	0	0		This issue is not mentioned in the Policy and so will be addressed by Development Control policy only.
		Will it increase waste recovery and recycling?	0	0	0		
	4.3 Limit or reduce vulnerability to the effects of climate change (including flooding)	Will it minimise risk to people and property from flooding, storm events or subsidence?	0	0	0		This issue is not mentioned in the Policy and so will be addressed by Development Control policy only. It is not likely to be a significant issue at this location.
5. Healthy communities	5.1 Maintain and enhance human health	Will it reduce substantially reduce mortality rates?	0	0	0	The Policy highlights the importance of ensuring complete remediation of any existing land contamination prior to any development.	
		Will it encourage healthy lifestyles, including travel choices?	0	0	0		

	5.2 Reduce and prevent crime, and reduce the fear of crime	Will it reduce actual levels of crime?	0	0	0	No significant effect
		Will it reduce fear of crime?	0	0	0	
	5.3 Improve the quantity and quality of publicly accessible open space	Will it increase the quantity and quality of publicly accessible open space?	0	0	0	This issue is not mentioned in the Policy and so will be addressed by Development Control policy only.
6. Inclusive communities	6.1 Improve the quality, range and accessibility of services and facilities (e.g. health, transport, education, training, leisure opportunities)	Will it improve the quality and range of services and facilities, including health, education, shopping, sport, leisure, arts and cultural activities?	0	0	0	As highlighted in the supporting text to the Policy, this site is well located in terms of local services and facilities and public transport, and as a result is suitable for a high density of development.
		Will it improve accessibility to key local services and facilities, including health, education and leisure (shops, post offices, pubs etc)?	+	+	+	
		Will it improve accessibility by means other than the car and improve the attractiveness of environmentally better modes including public transport, cycling and walking?	+	+	+	

		Will it support and improve community and public transport?	0	0	0	
6.2 Redress inequalities related to age, gender, disability, race, faith, location and income		Will it improve relations between people from different backgrounds or social groups?	0	0	0	No significant effects
		Will it reduce poverty and social exclusion in those areas most affected?	0	0	0	
		Will it promote accessibility for all members of society, including the elderly and disabled?	0	0	0	
6.3 Ensure all groups have access to decent, appropriate and affordable housing		Will it support the provision of a range of housing types and sizes, including affordable and key worker housing, to meet the identified needs of all sectors of the community?	+	+	+	Development Control policy will be used to ensure an appropriate mix of housing, including affordable housing.
		Will it reduce the number of unfit homes?	0	0	0	

		Will it meet the needs of the travelling community?	0	0	0	No significant effects.
	6.4 Encourage and enable the active involvement of local people in community activities	Will it increase the ability of people to influence decisions?	0	0	0	
		Will it encourage engagement with community activities?	0	0	0	
7.Economic activity	7.1 Help people gain access to satisfying work appropriate to their skills, potential and place of residence	Will it encourage businesses development?	0	0	0	The supporting text highlights that this location is well served by public transport into the City (a major employment location) and as a result it is a suitable location for a relatively high density development.
		Will it improve the range of employment opportunities to provide a satisfying job or occupation for everyone who wants one?	0	0	0	
		Will it improve accessibility to local employment by means other than the car?	+	+	+	
		Will it encourage the rural economy and diversification?	0	0	0	

	7.2 Support appropriate investment in people, places, communications and other infrastructure	Will it improve the level of investment in key community services and infrastructure?	0	0	0	No significant effect	
		Will it support provision of key communications infrastructure, including broadband?	0	0	0		
		Will it improve access to education and training, and support provision of skilled employees to the economy?	0	0	0		
	7.3 Improve the efficiency, competitiveness, vitality and adaptability of the local economy	Will it improve business development and enhance competitiveness?	0?	0?	0?		This development will result in the loss of a local, small scale business / employment location. However, it is not clear that effects will be significant enough to require a specific mention in the Policy.
		Will it support the Cambridge area's position as a world leader in research and technology based industries, higher education and research, particularly through the development and expansion of clusters?	0	0	0		



		Will it support sustainable tourism?	0	0	0	
		Will it protect the shopping hierarchy, supporting the vitality and viability of Cambridge City Centre, town, district, and local centres?	0	0	0	
<p><b>Summary:</b></p> <p>This is a small scale redevelopment within the village of Great Shelford. The Policy aims to ensure that the two major locational constraints will be adequately addressed as part of future development, namely the integrity of the conservation area and the potential issue of land contamination. It is possible that there may be some negative residual effects on the integrity of the conservation area, although good design will go a long way to mitigating effects and could even result in positive effects in terms of townscape. A high density of development is proposed to make the most of the fact that this is a brownfield location with good access to local services and facilities as well as public transport. Because of the relatively modest size of any potential development, the Policy is silent on all other issues, instead relying on Development Control Policies DPD policies to ensure that potential sustainability issues are addressed</p>						
<p><b>Recommendations:</b></p> <p>None</p>						

### Preferred Option Policy: Ida Darwin Hospital, Fulbourn

SA Topic	SA objectives	Decision-making criteria	Short term	Med term	Long term	Comments
1. Land and water resources	1.1 Minimise the irreversible loss of undeveloped land and productive agricultural holdings	Will it use land that has been previously developed?	+	+	+	The site contains a series of buildings set in landscaped grounds. The Policy and supporting text describe how the reconfiguration of the site can make best use of the site footprint.
		Will it use land efficiently?	+	+	+	
		Will it protect and enhance the best and most versatile agricultural land?	+	+	+	
	1.2 Reduce the use of non-renewable resources, including energy sources	Will it reduce emissions of greenhouse gases by reducing energy consumption?	?	?	?	It is stated that this development does not lend itself to being an exemplar development due to its small size, and so standards of sustainable design and construction will be governed by Development Control Policy. It may be that energy use in the home per capita is reduced as a result, but there remains some uncertainty.
		Will it lead to an increased proportion of energy and other resources being met from renewable sources?	?	?	?	
	1.3 Limit water consumption to	Will it reduce water consumption?	-	-	-	The absolute effect of further development will be to put additional strain on already

	levels supportable by natural processes and storage systems	Will it conserve ground water resources?	-	-	-	stretched water resources in the sub-region, therefore possibly leading to lowered groundwater levels and impacts on natural processes.  It is stated that this development does not lend itself to being an exemplar development due to its small size, and so standards of sustainable design and construction will be governed by Development Control Policy. Water efficiency per capita may increase but overall water consumption in the Cambridge Sub-region will not decrease.
2. Biodiversity	2.1 Avoid damage to designated sites and protected species	Will it protect sites designated for nature conservation interest?	0	0	0	No significant effects
	2.2 Maintain and enhance the range and viability of characteristic habitats and species	Will it conserve species, reversing declines, and help to enhance diversity?	+	+	+	The Policy refers to the requirement for a biodiversity strategy, and the supporting text highlights that the reversion of land to open countryside presents opportunities for biodiversity enhancement.
		Will it reduce habitat fragmentation?	+	+	+	
		Will it help achieve Biodiversity Action Plan targets?	+	+	+	
	2.3 Improve opportunities for people to access and appreciate wildlife and wild places	Will it improve access to wildlife, and wild places?	+	+	+	As highlighted in the supporting text, removal of buildings from the western part of the site will create an area of open countryside character (a green wedge) on the edge of Fulbourn village.
Will it maintain and, where possible, increase the area of high-quality green space in the District?		+	+	+		

		Will it promote understanding and appreciation of wildlife?	0	0	0	
		Will it improve access to the wider countryside through the network of public rights of way?	0	0	0	
3. Landscape, townscape and archaeology	3.1 Avoid damage to areas and sites designated for their historic interest, and protect their settings.	Will it protect or enhance sites, features of areas of historical, archaeological, or cultural interest (including conservation areas, listed buildings, registered parks and gardens and scheduled monuments)?	?	?	?	As highlighted in the Policy, Fulbourn Hospital has been designated as a conservation area, and so any development on this site will need to preserve and enhance its setting. The supporting text details the important elements of the conservation area and states that: <i>“Any additional development will need to consider the historic pattern of development and parkland settlement.”</i> It may be that this Policy will ensure no negative overall effects on the conservation area.
	3.2 Maintain and enhance the diversity and distinctiveness of landscape and townscape character	Will it maintain and enhance the diversity and distinctiveness of landscape and townscape character?	+	+	+	This site is located in the Green Belt, but as stated in the Policy: <i>“Re-development will create a green wedge on the western part of the Ida Darwin site to provide a compensatory enhancement to the openness of the Green Belt in this location.”</i> The supporting text gives details as to how the residential development will be
		Will it protect and enhance open spaces of amenity and recreational value?	+	+	+	

		Will it maintain and enhance the character of settlements?	+	+	+	designed and landscaped to minimise impact on the character of the Green Belt The supporting text considers the relationship of the site with Fulbourn village, and states that: <i>“Redevelopment of the built footprint in a different configuration as a residential area would change the character of the site and its relationship with Fulbourn, but there is potential for this to have positive implications for the relationship of the site with Fulbourn village and for the Green Belt by the removal of all buildings from the western part of the site...”</i>
	3.3 Create places, spaces and buildings that work well, wear well and look good	Will it improve the satisfaction of people with their neighbourhoods as places to live?	0	0	0	The Policy states that: <i>“A Development Brief for this sensitive location in the Green Belt between Cambridge and Fulbourn is required to be submitted to and approved by the Local Planning Authority prior to granting of planning permission.”</i>
		Will it lead to developments built to a high standard of design, and good place making?	+	+	+	
4. Climate change and pollution	4.1 Reduce emissions of greenhouse gasses and other pollutants (including air, water, soil, noise, vibration and light)	Will it reduce emissions of greenhouse gases?	?	?	?	The site is located relatively close to Cambridge and other employment sites and offers the potential for sustainable travel by both bus and cycle. However, there is no mention in the Policy of making the most of these locational opportunities. It is therefore unclear whether there is the potential for this development to allow
		Will it improve air quality?	0	0	0	
		Will it reduce traffic volumes?	0	0	0	

		Will it support travel by means other than the car?	?	?	?	<p>people to live more sustainably, with a smaller carbon footprint.</p> <p>Noise and vibration from the railway line is an important consideration, which is referred to in the Policy and supporting text. It may be possible to effectively mitigate effects.</p> <p>The supporting text recognises that this site is within a groundwater protection zone and appropriate measures would therefore need to be provided so as to ensure the water environment is protected from contamination. It is therefore likely that diffuse and point source water pollution can be minimised, although it is unclear whether water quality can be improved.</p>
		Will it reduce levels of noise or noise concerns?	?	?	?	
		Will it reduce or minimise light pollution?	0	0	0	
		Will it improve water quality including by reducing diffuse and point source water pollution?	?	?	?	
	4.2 Minimise waste production and support the recycling of waste products	Will it reduce household waste?	0	0	0	This issue is not mentioned in the Policy and so will be addressed by Development Control policy only.
		Will it increase waste recovery and recycling?	0	0	0	
	4.3 Limit or reduce vulnerability to the effects of climate change (including flooding)	Will it minimise risk to people and property from flooding, storm events or subsidence?	+	+	+	The supporting text refers for the need for a site specific flood risk assessment as a result of high groundwater levels. It is therefore possible that effects can be minimised.
5. Healthy communities	5.1 Maintain and enhance human health	Will it reduce substantially reduce mortality rates?	0	0	0	The Policy and supporting text describe the importance of adequate remediation of any contaminated land.

		Will it encourage healthy lifestyles, including travel choices?	+	+	+	Provision of high quality open space and opportunities for walking and cycling should lead to benefits in terms of promoting healthy lifestyles.
	5.2 Reduce and prevent crime, and reduce the fear of crime	Will it reduce actual levels of crime?	0	0	0	No significant effect
		Will it reduce fear of crime?	0	0	0	
	5.3 Improve the quantity and quality of publicly accessible open space	Will it increase the quantity and quality of publicly accessible open space?	+	+	+	This reconfiguration of the Green Belt will create an accessible and high quality green wedge.
6. Inclusive communities	6.1 Improve the quality, range and accessibility of services and facilities (e.g. health, transport, education, training, leisure opportunities)	Will it improve the quality and range of services and facilities, including health, education, shopping, sport, leisure, arts and cultural activities?	+	+	+	<p>The supporting text describes the plans of relocating some of the healthcare services from the Ida Darwin site to the Fulbourn Hospital site. It is stated that:</p> <p><i>“the total [number of dwellings] would depend on any buildings that would remain on the Ida Darwin site, and the amount of additional healthcare development required on the Fulbourn Hospital site.”</i></p> <p>It can therefore be seen that the needs of the healthcare provider are being taken into account.</p> <p>The site is located relatively close to Cambridge and other employment sites and</p>
		Will it improve accessibility to key local services and facilities, including health, education and leisure (shops, post offices, pubs etc)?	+	+	+	

		Will it improve accessibility by means other than the car and improve the attractiveness of environmentally better modes including public transport, cycling and walking?	+	+	+	offers the potential for sustainable travel by both bus and cycle. There is no mention in the Policy of making the most of locational opportunities in terms of accessibility by sustainable modes of transport, although this will be addressed through Development Control policy.
		Will it support and improve community and public transport?	0	0	0	
6.2 Redress inequalities related to age, gender, disability, race, faith, location and income		Will it improve relations between people from different backgrounds or social groups?	0	0	0	No significant effects
		Will it reduce poverty and social exclusion in those areas most affected?	0	0	0	
		Will it promote accessibility for all members of society, including the elderly and disabled?	0	0	0	



	6.3 Ensure all groups have access to decent, appropriate and affordable housing	Will it support the provision of a range of housing types and sizes, including affordable and key worker housing, to meet the identified needs of all sectors of the community?	+	+	+	Development Control policy will be used to ensure an appropriate mix of housing, including affordable housing.	
		Will it reduce the number of unfit homes?	0	0	0		
		Will it meet the needs of the travelling community?	0	0	0		
	6.4 Encourage and enable the active involvement of local people in community activities	Will it increase the ability of people to influence decisions?	0	0	0		No significant effects
		Will it encourage engagement with community activities?	0	0	0		
	7.Economic activity	7.1 Help people gain access to satisfying work appropriate to their skills, potential and place of residence	Will it encourage businesses development?	0	0		0
Will it improve the range of employment opportunities to provide a satisfying job or occupation for everyone who wants one?			0	0	0		

		Will it improve accessibility to local employment by means other than the car?	0	0	0	
		Will it encourage the rural economy and diversification?	0	0	0	
	7.2 Support appropriate investment in people, places, communications and other infrastructure	Will it improve the level of investment in key community services and infrastructure?	0	0	0	This issue is not mentioned in the Policy and so will be addressed by Development Control policy only.
		Will it support provision of key communications infrastructure, including broadband?	0	0	0	
		Will it improve access to education and training, and support provision of skilled employees to the economy?	0	0	0	
	7.3 Improve the efficiency, competitiveness, vitality and	Will it improve business development and enhance competitiveness?	0	0	0	This is a residential-led development that will provide workers to support the growth of the Cambridge economy.

adaptability of the local economy	Will it support the Cambridge area's position as a world leader in research and technology based industries, higher education and research, particularly through the development and expansion of clusters?	+	+	+
	Will it support sustainable tourism?	0	0	0
	Will it protect the shopping hierarchy, supporting the vitality and viability of Cambridge City Centre, town, district, and local centres?	0	0	0

**Summary:**

This Policy has a focus on addressing the key locational constraint, which is the presence of the site within the Cambridge Green Belt. Development at this site, as guided by this Policy can be considered as a re-configuration of the Major Developed Site in the Green Belt, as a green wedge will be created where there is currently built development, providing compensatory open space with a countryside character. The Policy promotes the opportunity for the green wedge to lead to biodiversity gain and improved access to green space for the residents of Fulbourn village. The Policy refers to the requirement for a biodiversity strategy, and the supporting text highlights that the reversion of land to open countryside presents opportunities for biodiversity enhancement. The Policy and/or supporting text also refer to other key constraints to development at this location, namely land contamination, flood risk, the designated conservation area and noise and vibration from the nearby railway. There area unlikely to be any significant residual negative sustainability effects associated with these constraints, assuming that the Policy and other Development Control Policies DPD policies are fully implemented.

Even though this is not a small development, there are a number of other issues that are not elaborated on in the Policy, but rather Development Control Policies DPD policies will be utilised to ensure that there are no negative sustainability impacts and that sustainability benefits are maximised.

**Recommendations:**

None

### Preferred Option Policy: Arbury Park, Cambridge

SA Topic	SA objectives	Decision-making criteria	Short term	Med term	Long term	Comments
1. Land and water resources	1.1 Minimise the irreversible loss of undeveloped land and productive agricultural holdings	Will it use land that has been previously developed?	+	+	+	The site was formerly agricultural land, but has since been cleared for development. The Policy does not consider the density of development that might be appropriate, other than stating that the site has been subject to a design led approach which take account of the constraints of the site.
		Will it use land efficiently?	?	?	?	
		Will it protect and enhance the best and most versatile agricultural land?	+	+	+	
	1.2 Reduce the use of non-renewable resources, including energy sources	Will it reduce emissions of greenhouse gases by reducing energy consumption?	?	?	?	The Policy does not give consideration to required standards of sustainable design and construction beyond stating that the development will be 'sustainable', and so it is assumed that standards of sustainable design and construction will be governed by Development Control policy. It may be that energy use in the home per capita is reduced as a result, but there remains some uncertainty.
		Will it lead to an increased proportion of energy and other resources being met from renewable sources?	?	?	?	
	1.3 Limit water consumption to	Will it reduce water consumption?	-	-	-	The absolute effect of further development will be to put additional strain on already stretched

	levels supportable by natural processes and storage systems	Will it conserve ground water resources?	-	-	-	water resources in the sub-region, therefore possibly leading to lowered groundwater levels and impacts on natural processes. The Policy does not give consideration to specific water conservation measures. These issues will be addressed by other Development Control Policies. Water efficiency per capita may increase but overall water consumption in the Cambridge Sub-region will not decrease.
2. Biodiversity	2.1 Avoid damage to designated sites and protected species	Will it protect sites designated for nature conservation interest?	0	0	0	No significant effect
	2.2 Maintain and enhance the range and viability of characteristic habitats and species	Will it conserve species, reversing declines, and help to enhance diversity?	+	+	+	The Policy promotes: <i>“The retention of appropriate existing features of ecological interest and the creation of new features which will enhance the interest of the site.”</i>
		Will it reduce habitat fragmentation?	+	+	+	
		Will it help achieve Biodiversity Action Plan targets?	+	+	+	
	2.3 Improve opportunities for people to access and appreciate wildlife and wild places	Will it improve access to wildlife, and wild places?	0	0	0	The Policy promotes strengthening the links to the neighbouring rural area to the north of the A14.
Will it maintain and, where possible, increase the area of high-quality green space in the District?		0	0	0		

		Will it promote understanding and appreciation of wildlife?	0	0	0	
		Will it improve access to the wider countryside through the network of public rights of way?	+	+	+	
3. Landscape, townscape and archaeology	3.1 Avoid damage to areas and sites designated for their historic interest, and protect their settings.	Will it protect or enhance sites, features of areas of historical, archaeological, or cultural interest (including conservation areas, listed buildings, registered parks and gardens and scheduled monuments)?	+	+	+	The Policy gives specific consideration to achieving the in-situ preservation or enhancement of the Arbury Camp site of archaeological interest. It is likely therefore that negative effects can be avoided and the site protected in-situ in perpetuity.
	3.2 Maintain and enhance the diversity and distinctiveness of landscape and townscape character	Will it maintain and enhance the diversity and distinctiveness of landscape and townscape character?	?	?	?	The Policy gives detailed attention to the retention of an attractive urban edge to Cambridge through the use of high standards of design and landscaping and the creation of gateway features. It gives particular attention to ensuring that noise attenuation measures do not impact upon the landscape. However, it is impossible to say at this stage that there will not be significant effects.
		Will it protect and enhance open spaces of amenity and recreational value?	0	0	0	

		Will it maintain and enhance the character of settlements?	0	0	0	
	3.3 Create places, spaces and buildings that work well, wear well and look good	Will it improve the satisfaction of people with their neighbourhoods as places to live?	0	0	0	The Policy promotes high standards of design and landscaping and the creation of 'gateway features'.
		Will it lead to developments built to a high standard of design, and good place making?	+	+	+	
4. Climate change and pollution	4.1 Reduce emissions of greenhouse gasses and other pollutants (including air, water, soil, noise, vibration and light)	Will it reduce emissions of greenhouse gases?	+	+	+	The Policy states that the masterplan must provide for <i>"Maximum penetration and service of the site by public transport, including the extension of existing bus routes and full utilisation of the potential of direct connection to any future public transport route along the former railway line; and the creation of strong internal cycle and footpath links between component parts of the development and the retention and strengthening of such links to neighbouring parts of the urban area and to the rural area to the north of the A14"</i> As a result of this Policy, it is felt that there is the potential for this development to allow people to live more sustainably, with a smaller carbon footprint.
		Will it improve air quality?	?	?	?	
		Will it reduce traffic volumes?	?	?	?	
		Will it support travel by means other than the car?	+	+	+	
		Will it reduce levels of noise or noise concerns?	?	?	?	

		Will it reduce or minimise light pollution?	0	0	0	<p>The potential to increase traffic congestion, as well as impacts from air and noise pollution are serious constraints at this site.</p> <p>The Policy has a major focus on the provision of adequate attenuation measures in relation to noise and emissions generated by traffic on the A14, including the adoption of an appropriate layout and disposition of uses.</p> <p>An Air Quality Assessment is required to be submitted along-side planning applications that takes account of the anticipated effects of the A14 Ellington to Fen Ditton Improvements on the nature and extent of the A14 AQMA.</p> <p>The supporting text has a significant focus on ensuring that specific local traffic constraints are not worsened. A Transport Assessment will be required that takes account of the net impact of replacing parcels previously assumed for commercial development with residential development.</p> <p>However, effects have been scored as uncertain as there may still be the potential for residual effects.</p>
		Will it improve water quality including by reducing diffuse and point source water pollution?	0	0	0	
4.2 Minimise waste production and support the recycling of waste products		Will it reduce household waste?	0	0	0	<p>This issue is not mentioned in the Policy and so will be addressed by Development Control policy only.</p>
		Will it increase waste recovery and recycling?	0	0	0	



	4.3 Limit or reduce vulnerability to the effects of climate change (including flooding)	Will it minimise risk to people and property from flooding, storm events or subsidence?	0	0	0	This issue will be addressed by Development Control policy only. It is not expected to be a major issue at this site.
5. Healthy communities	5.1 Maintain and enhance human health	Will it substantially reduce mortality rates?	0	0	0	The Policy promotes designing in opportunities for walking and cycling.
		Will it encourage healthy lifestyles, including travel choices?	+	+	+	
	5.2 Reduce and prevent crime, and reduce the fear of crime	Will it reduce actual levels of crime?	0	0	0	This issue will be addressed by Development Control policy only.
		Will it reduce fear of crime?	0	0	0	
	5.3 Improve the quantity and quality of publicly accessible open space	Will it increase the quantity and quality of publicly accessible open space?	+	+	+	The Policy gives consideration to open space provision, including through stating that: <i>“Consideration may be given to off-site provision of the active recreational needs of new residents in consultation with Impington Parish Council or the proposed Orchard Park Community Council as the managing agency.”</i> It also promotes using the Arbury Camp area of archaeological interest as public open space.

6. Inclusive communities	6.1 Improve the quality, range and accessibility of services and facilities (e.g. health, transport, education, training, leisure opportunities)	Will it improve the quality and range of services and facilities, including health, education, shopping, sport, leisure, arts and cultural activities?	+	+	+	The Policy promotes delivery of a local centre as well as a high level of accessibility by walking and cycling and good access to public transport.
		Will it improve accessibility to key local services and facilities, including health, education and leisure (shops, post offices, pubs etc)?	+	+	+	
		Will it improve accessibility by means other than the car and improve the attractiveness of environmentally better modes including public transport, cycling and walking?	+	+	+	
		Will it support and improve community and public transport?	0	0	0	
	6.2 Redress inequalities related to age, gender, disability, race, faith, location and income	Will it improve relations between people from different backgrounds or social groups?	0	0	0	No significant effect

		Will it reduce poverty and social exclusion in those areas most affected?	0	0	0	
		Will it promote accessibility for all members of society, including the elderly and disabled?	0	0	0	
	6.3 Ensure all groups have access to decent, appropriate and affordable housing	Will it support the provision of a range of housing types and sizes, including affordable and key worker housing, to meet the identified needs of all sectors of the community?	+	+	+	Development Control policy will be used to ensure an appropriate mix of housing, including affordable housing.
		Will it reduce the number of unfit homes?	0	0	0	
		Will it meet the needs of the travelling community?	0	0	0	
	6.4 Encourage and enable the active involvement of local people in community activities	Will it increase the ability of people to influence decisions?	0	0	0	No significant effect
		Will it encourage engagement with community activities?	0	0	0	

7.Economic activity	7.1 Help people gain access to satisfying work appropriate to their skills, potential and place of residence	Will it encourage businesses development?	+	+	+	<p>The Policy allows up to 18,000 m<sup>2</sup> of business development, although no further guidance is given as to the type of business activity that should be promoted or how it should be integrated within the development.</p> <p>The changes to the policy provide guidance regarding additional residential development beyond the 900 dwellings. If additional land is used for residential development this will reduce the land available for commercial development.</p>
		Will it improve the range of employment opportunities to provide a satisfying job or occupation for everyone who wants one?	+	+	+	
		Will it improve accessibility to local employment by means other than the car?	+	+	+	
		Will it encourage the rural economy and diversification?	0	0	0	
	7.2 Support appropriate investment in people, places, communications and other infrastructure	Will it improve the level of investment in key community services and infrastructure?	0	0	0	This issue will be addressed by Development Control policy only.
		Will it support provision of key communications infrastructure, including broadband?	0	0	0	

		Will it improve access to education and training, and support provision of skilled employees to the economy?	0	0	0	
7.3 Improve the efficiency, competitiveness, vitality and adaptability of the local economy		Will it improve business development and enhance competitiveness?	0	0	0	<p>The Policy will deliver housing for workers who can support the growth of Cambridge.</p> <p>The Policy promotes a 'local centre' only, and so it is not thought that the shopping hierarchy will be adversely affected.</p>
		Will it support the Cambridge area's position as a world leader in research and technology based industries, higher education and research, particularly through the development and expansion of clusters?	+	+	+	
		Will it support sustainable tourism?	0	0	0	
		Will it protect the shopping hierarchy, supporting the vitality and viability of Cambridge City Centre, town, district, and local centres?	+	+	+	

**Summary:**

This is a Policy to guide a large mixed-use development. The Policy promotes making the most of opportunities for sustainable transport and ecological enhancement. It takes a range of important constraints into detailed account, particularly the mitigation of noise and emissions from the A14, the need to protect the landscape setting Cambridge, the need to minimise traffic congestion and the need to protect the Arbury Camp site of archaeological interest. There are significant local constraints in terms of traffic congestion and associated noise and air quality impacts. Implementation of the Policy will go some distance to mitigating effects, and it is likely that the A14 improvements will improve the situation, but it will be important to monitor to ensure no residual effects, particularly in terms of health the health of residents.

Even though this is a large development, there are a number of other issues that are not elaborated on in the Policy, but rather Development Control Policies DPD policy only will be utilised to ensure that there are no negative sustainability impacts and that sustainability benefits can be maximised.

**Recommendations:**

None

## Appendix IV: SCDC response to suggested policy changes

### **Preferred Option Policy: Land between Huntingdon Road, Histon Road and the A14, North West Cambridge (South Cambs Revision)**

#### **Recommendation 1 - Clarify why a 'balance may need to be struck' in terms of the provision of affordable housing.**

SCDC Officer Response: The NIAB development is a major and complex development which has a wide variety of requirements covering infrastructure and services, and a balance will need to be struck between competing requirements, in the light of economic viability. Such a policy has been found sound in relation to other major developments in the District.

No change.

#### **Recommendation 2 - Demonstrate that services and facilities will be adequately accessible for all members of society, including the elderly and disabled, from the day that people move into the new development.**

SCDC Officer Response: This point is addressed by Policy DP/3 of the Development Control Policies DPD, that would apply to the site, which requires, 'safe and convenient access for all to public buildings and spaces, and to public transport, including those with limited mobility, or those with other impairment such as of site or hearing.'

No change.

#### **Recommendation 3 - Add clarity to this requirement for exemplar developments.**

SCDC Officer Response: The Council considers that this element of the policy is sound. Similar policies have been used in Area Action Plans for other major developments. The exact measures to be included can be agreed through the masterplanning process.

No change.